



Quality information

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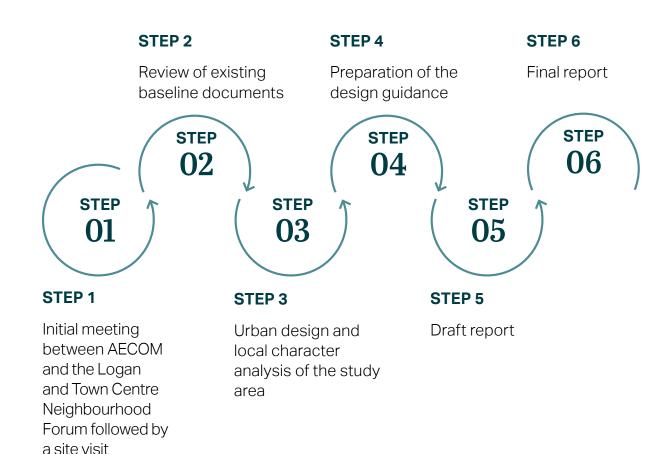
1. Introduction

1.1 Purpose and process

The guidance set out in this report will provide a detailed framework that should be followed by any future design proposals that come forward within the neighbourhood area to ensure it meets a consistent, high-quality standard of design and positively contributes to the unique characteristics of Logan and Town Centre.

It is intended that this report becomes an integral part of the Neighbourhood Plan by informing policies that will influence the design of new development and have weight in the planning process.

The following steps were agreed with the Neighbourhood Forum to produce this report, which draws upon policy development and engagement work undertaken by the Group:



1.2 How to use this document

This document has set out an evidence base for the Logan and Town Centre Neighbourhood Plan and it is recommended that the guidance and codes are embedded within the forthcoming plan as policy.

As well as providing certainty to the local community, the design guidance and codes in this document should give more certainty to applicants, as they will be able to design a scheme that is reflective of community aspirations.

The document will be used by a range of potential users in the following ways:

Potential users	How they will use the design guidance	
Applicants, developers, & landowners	As a guide to the community's and the local planning authority's expectations on design, allowing a degree of certainty – they will be expected to follow the Guidelines as planning consent is sought.	
Local planning authority	As a reference point, embedded in policy, against which to assess planning applications. The guidance should be discussed with applicants during any pre application discussions.	
Logan and Town Centre Neighbourhood Forum	As a guide when commenting on planning applications, ensuring that the guidance is complied with.	
Local community organisations	As a tool to promote community-backed development and to inform comments on planning applications.	

Table 01: A list of potential users of this documents and how they will apply the design guidance and codes.

1.3 How this document relates to the other Market Harborough Neighbourhood Plan Design Guides

Market Harborough is split into four separate neighbourhood forums, each developing its own design guidance and codes. Understandably there are similarities between the four areas. They are part of the same town and therefore have similar characteristics.

For this reason, this document is structured firstly with analysis of the historic evolution of Market Harborough as a whole before looking at the context of the neighbourhood area that the guide is for. After this there is a set of Market Harborough wide design guidelines which are mostly similar across the four different neighbourhood forums.

In addition, as part of the analysis undertaken for this exercise, Market Harborough was split into 10 different area types representing areas of the town with similar characteristics. While certain area types appear in different wards, each of these areas do have opportunities, constraints and finer details which put them apart from places that fall within the same area type in a different part of the town. Given this, each design guide will contain guidance that is bespoke to its character and needs in the fifth chapter of the documents.

This approach allows for a balance of having four documents that are consistent enough for it to be clear that they all form a part of the same wider town strategy, at the same time as ensuring that the identity of each neighbourhood area is respected and enhanced.



1.4 Policy context

This section outlines the national and local planning policy and guidance documents which should be read in conjunction with this design guide.



National planning documents

2024 - National Planning Policy Framework (NPPF)

The NPPF sets out the nationwide planning policies and government's expectations on how these should be applied. The NPPF requires all local planning authorities to prepare design guides or codes consistent with the national guidance documents. Previous NPPF updates introduced references to the National Design Guide and National Model Design Code and the use of area, neighbourhood, and site-specific design guides, in order to ensure good quality and context sensitive design.

2021 - National Model Design Code

The National Model Design Code is an established part of the government's guidance on planning and should be read alongside the National Design Guide. The National Model Design Code provides guidance on the production of design codes, guides, and policies to promote well-designed places. It sets out the key design parameters that need to be considered when producing design guides and recommends methodology for capturing and reflecting views of the local community.

2021 - National Design Guide

The National Design Guide sets out the government's ten priorities for well-designed places and illustrates how well-designed places can be achieved in practice. The ten characteristics identified includes: context, identity, built form, movement, nature, public spaces, uses, homes and buildings, resources, and lifespan.

2020 - Building for a Healthy Life

Building for a Healthy Life updates Homes England's key measure of design quality as the national housing accelerating body. The document sets out 12 considerations for creating integrated neighbourhoods, distinctive places, and streets for all. While it is not part of the national policy, it is recognised as best practice guidance and design tool in assessing the quality of design proposals.

2007 - Manual for Streets

Development is expected to respond positively to the Manual for Streets, the Government's guidance on how to design, construct, adopt and maintain new and existing residential streets. It promotes development that avoids car dominated layouts in favour of pedestrians and cyclists.

Regional policy and guidance

Harborough Local Plan 2011 - 2031 - April 2019

Harborough District Council (HDC) is the Local Planning Authority (LPA) for Logan and Town Centre. HDC adopted the Local Plan in April 2019. In the adopted Local Plan, Market Harborough was considered a sub-regional centre, with commitments of 3,100 sam of convenience retail space, 8,000 sgm of comparison retail space, with 1,500 homes committed through a strategic development to northwest of the town, alongside 9.5 hectares (ha) of employment uses allocated. Across Market Harborough, further development was allocated in the amount of 1.078 dwellings on three major housing sites, and an additional 13 ha employment allocation. Three site allocations were made at Overstone Park for 600 dwellings. east of Blackberry Grange Northampton Road for 350 dwellings and Burnmill Farm for 128 dwellings. Employment was allocated at Land at Airfield Farm for 13 hectares. Airfield Business Park for 6 hectares, and Compass Point Business Park for 5 hectares.

The LPA flags that Market Harborough must avoid coalescence with neighbouring settlements especially Lubenham and Great Bowden, hence it assigns strategic separation gaps between the settlements.

Harborough Emerging Local Plan

At the time of writing, the Emerging Local Plan is out to the formal Regulation 19 consultation. The draft plan allocates 1,350 homes and 4.9ha of employment land in Market Harborough. The town is assigned the second tier in the settlement hierarchy as a Market Town. The draft plan includes a large scale urban extension to the north of the town

Planning Obligations Supplementary Planning Document (SPD) - 2022

This SPD was adopted in June 2022. The document sets out the approach HDC takes to securing community infrastructure and affordable housing through the planning obligations process

Development Management SPD - 2021

This SPD was adopted in December 2021 and provides guidance to assist with the interpretation and implementation of the Local Plan. It covers important guidance on the design of new residential housing estates, extensions to homes, conversions, shopfronts and advertisements. This design guidance should be consulted alongside this document. This document adds further detail specific to the NA.

Market Harborough Transport Strategy - 2016

This document sets out 18 recommendations for transport mitigation measures within and around Market Harborough. These include mitigation measures for capacity/congestion improvements, changes to the network and traffic routing, sustainable transport initiatives, safety improvements, traffic management improvements, HGV controls and highway maintenance.

Market Harborough Town Centre Masterplan - 2022

This masterplan was adopted in June 2022 and is a vision for the town centre. The remit of the document is to ensure the future viability and vitality of the town centre. It sets out several improvements to public realm infrastructure in the town centre. The document only covers the immediate town centre retail area and not the whole neighbourhood area.

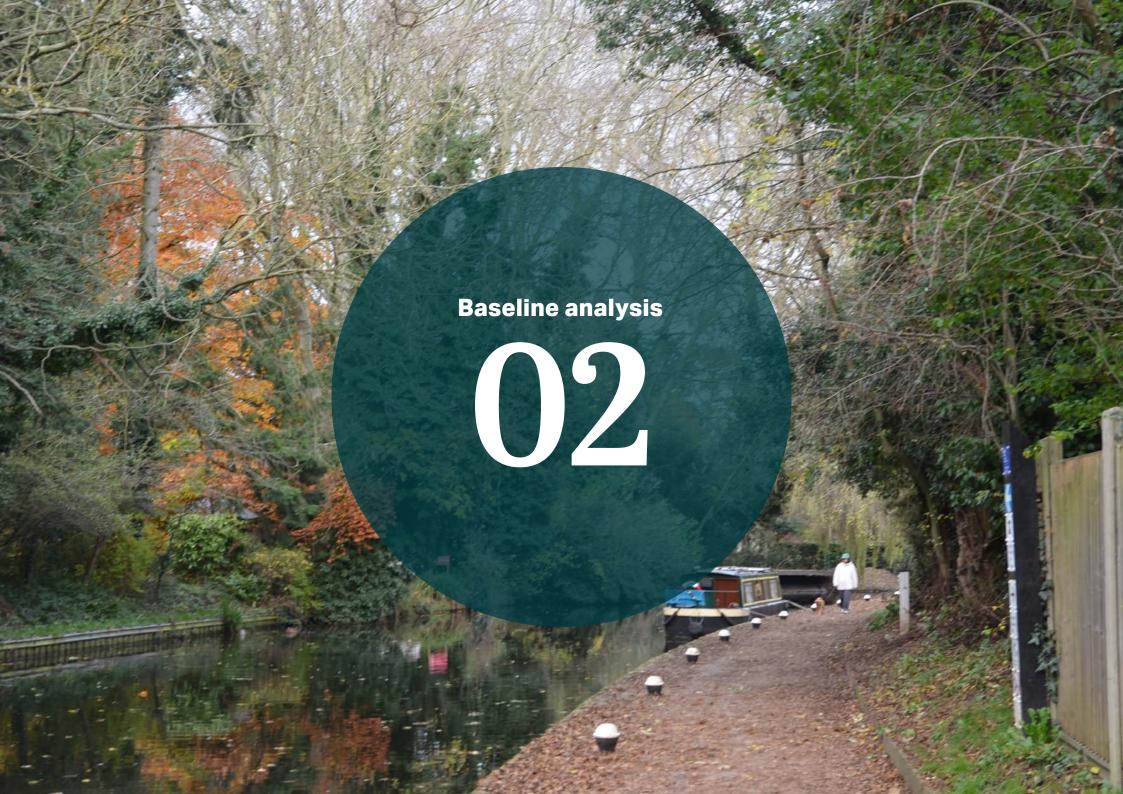
1.5 Area of study

Market Harborough is a market town located in the district of Harborough in the county of Leicestershire. It is located on the border of Leicestershire with Northamptonshire to the immediate south. Along with Lutterworth, it is one of the most important settlements within Harborough District.

The town recorded a population of 24,171 residents at the 2021 Census. Market Harborough has developed from a historic market town servicing a wide rural area to a modern urban area with a variety of industries, services and amenities. The town was influenced by the coaching industry, as it was located on an important route between London and Leicester. The Grand Union Canal and railway brought waves of industry, with many factories in the town creating employment through the late 19th century.

The town remains an important retail hub for the surrounding area. The town has been subdivided into four Neighbourhood Areas - Arden, Little Bowden, Logan and Town Centre, and Welland.





2. Baseline analysis

This section presents the historic evolution of Market Harborough as a whole and a snapshot of the Neighbourhood Area today to inform the design objectives of the design guidance. It provides an overview of Logan and Town Centre's heritage, landscape, movement network, land use and built form.

2.1 Settlement pattern and character

2.1.1 Market Harborough's historic evolution

Market Harborough emerged in medieval times from a rural, agricultural area with scattered hamlets. The area was historically known as Bowden and the Domesday Book records three hamlets: Great Bowden, Arden and Little Bowden, of 73 manors. Harborough emerged as an important road junction on the roads between London, Northampton and Leicester. A market was established in 1204, and this gave the town its prefix. The town owed its success to the market and the passing coaching traffic. For many centuries the town occupied a fairly small but busily packed linear area.

The arrival of the canal and railways had a transformative impact on the town because they simultaneously caused the decline of the traditional coaching industry but encouraged rapid industrialisation.

The canal branch was built in 1809 with a wharf to the north of the town centre, providing access to London and Birmingham via the Grand Union Canal. Railways development began in 1840, with different lines offering access to Leicester, Northampton, Rugby and London. The town became a key railway junction for the Midlands, and a halt on the Midland Main Line from London St Pancras to Sheffield.

The Symington Corset Factory began operation in 1876. This was followed in the 1890s by the opening of the Harborough Rubber Company, Looms Wooden Heels Factory and a tannery. The Caxton Works foundry began manufacturing in 1898. By this time the urban area rapidly expanded beyond the medieval core, with terraced housing built to the south along Northampton Road. Further development took place in Arden and Little Bowden, and the villages became more integrated into an urban area. Meanwhile, large scale residential development took place at the New Harborough Estate off Coventry Road.

The inter-war period saw a significant amount of speculative ribbon development along the main roads, and improvements to access and recreation facilities. Welland Park Road and Lubenham Road are examples of inter-war development. The town experienced substantial demolition of older overcrowded housing in the town centre, and major urban expansion into the countryside.

The post war period saw a further surge in the town's urban area as large scale local authority housebuilding took place at Bowden Fields Estate and the Southern Estate. In 1968 the town centre was declared a Conservation Area. The town was bypassed by all its arterial main roads in the 1990s. A shopping centre at St Mary's Place was opened in 1993. Since the start of the 21st century Market Harborough has continued to grow, with new residential estates built to the southeast near the Leisure Centre, and to the west and northwest, where thousands of additional homes have come forward.

Historic timeline

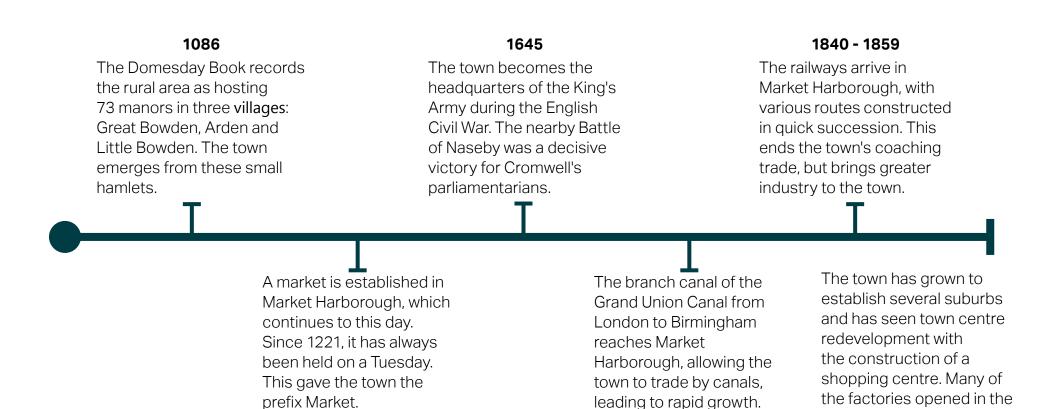


Figure 02: Market Harborough historic timeline.

1204

AECOM 13

1809

19th century have closed.

However, there are thriving

2025

employment areas.

2.2 Logan and Town Centre Historic Assets

Logan and the town centre are places that are both steeped in history. The town centre of Market Harborough is a focal point of historical interest, with a blend of Georgian, Victorian and other earlier architectural styles, the town retains much of its original charm due to its conservation area. Currently there are around 80 listed buildings within the neighborhood area, some of the key features include the Old Grammar School, an elevated timber-framed building dating back to 1614. St Dionysius Church is Grade I listed and is in the heart of the town, with an impressive spire that is a view that is valued by the community. The town centre also features a variety of Grade Il and Grade II* listed buildings, including former private residences, many of which are now repurposed for modern use, but the historic facades remain.

The Conservation Area encompasses the historic core of the market town. Its origins trace back to the lat 12th century when the town began to developing by the River Welland. The designation protects the historic integrity of the town, covering the central core and several surrounding streets. This area is protected from any development that does not respect or is not sympathetic to the character or scale of the historic core.

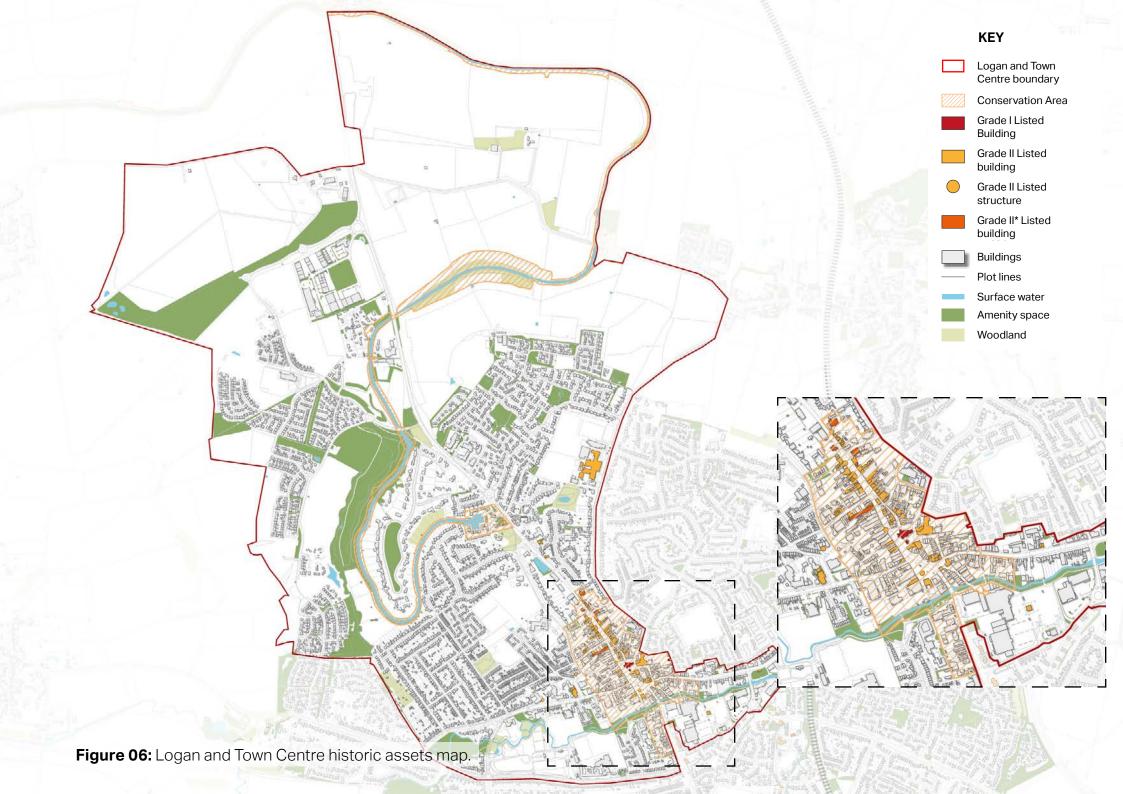
The Grand Union Canal flows through Logan and carries significant historical importance for the town's past industrial legacy, hence its status as a designated Conservation Area. It was first opened in 1809 as a branch of the Leicestershire and Northamptonshire Union Canal, which then later became part of the Grand Union Canal system. Its primary use was to connect the town to the national canal network during the Industrial Revolution for goods and services. The canal terminates at Union Wharf which is a site that was once incredibly active as a loading point for timber, coal and agricultural produce, meaning Market Harborough began to thrive as a commercial hub. Now it has been redeveloped as a marina offering new, more contemporary services.



Figure 03: Grade I listed St Dionysius' Church and Grade II* listed Old Grammar School.



Figure 04: Grand Union Canal at the Union Wharf canal basin.



2.3 Land Use

Logan and the Town Centre feature a diverse mix of land uses that cater for both its residents and also its visitors.

Primarily residential, there is a strong mix of housing styles throughout Logan, varying from flats and bungalows, terraced Victorian properties, and large detached houses, accommodating a wide demographic including families, young professionals, and retirees.

There are also many commercial uses that are scattered throughout Logan, however, predominantly in the town centre. The main retail core for the town is concentrated along High Street, with several smaller streets and 'courts' off to each side. There is a mix of both chain and independent shops, along with restaurants, cafes, salons and offices. Aside from the High Street, St Mary's Place a pedestrianised shopping arcade located in close proximity to River Welland, and Harborough Indoor Market, also have retail offering to the town. Beyond the town centre is Union Wharf to the north of Logan, which is a local hub of businesses. Other community assets in Logan include the Market Harborough Cricket and Squash Club and Stevens Street Allotments.

Welland Park is the largest park in Market Harborough and is a well used hub of activity for both residents and visitors alike. The park includes a variety of amenities as well as hosting a weekly park run and 'Healthy Walks' twice a week.

Logan has three primary schools and three secondary schools, all of which support the needs of local families. They are typically large buildings on large plots with generous green spaces acting as private playing fields for students. The private secondary school - Brooke House College is an exception to this, with buildings and boarding houses dispersed across the town centre. Its premises are usually demarcated by doors that are painted blue.

In the northern area of Logan there is a large industrial site, which was established in the early 21st century and is characterised by large scale warehouses. The warehouses are occupied by various companies for things such as publishing and joinery, as well as car tyre selling too. Just north of the industrial site is the Market Harborough Showground which is intermittently used for a variety of events, such as family festivals.



Figure 07: Shops along the High Street, with residential units on upper floors.



Figure 08: St Mary's Place shopping arcade.



Figure 09: Union Wharf apartments, businesses and marina.

2.4 Built Form

Logan and Town Centre showcase a diverse range of building typologies, reflecting various eras of residential development over time. Different periods of development come with different distinctive housing types and architectural styles.

New Harborough, a residential area in Logan, is defined by its narrow streets lined with Victorian and Edwardian terraced houses, most of which are red brick, with clay or S-tile roofs and a classic bay window on the ground floor frontage of the property. Due to the narrow roads there is very little off road parking provision, consequently causing a lot of congestion issues. West of Burnmill Road, down to Leicester Road, larger Victorian-era housing can also be found mixed in with post-war developments. These post-war homes vary in size and style, with terraced, semi-detached properties and bungalows. Unlike the Victorian homes, these are set back further from the street and often include on-plot parking.

Housing in The Woodlands and Union Wharf area are very different. In the former, they are mostly large detached properties formed around a central green space, sitting in expansive plots many of which have their gardens backing onto the canal. The canal itself leads down to Union Wharf where historic warehouses have been converted into apartments and restaurants alongside newer apartments, all built to remain inkeeping with the Wharf's past industrial charm.

The town centre is made up of predominantly Georgian and Victorian buildings with some 20th Century infill, all of which replaced Medieval buildings in their respective time periods. Some of the original long narrow burgage plots are still evident in the town centre's urban fabric, with some of these backlands brought back to active use in the form of courtyards (e.g. Bennetts Place Courtyard). But many of these plots have since been amalgamated to form larger plots that are fit for modern day business uses.

In the more modern estates, like Wellington Place Estate and Lubenham View Estate, there a mix of 3 and 4 bedroom houses, with some apartments. However, the style of these tend to be more homogeneous in areas of higher density.



Figure 10: Rows of Victoria terraces along Logan Street in New Harborough.



Figure 11: Canal-side housing in The Woodland.

2.5 Green and Blue Infrastructure

Logan and Town Centre has a rich network of green and blue infrastructure that significantly enhances the neighborhood area. Key water features include River Welland, which runs along the southern edge of the Neighbourhood Area through the Town Centre and Welland Park. The Grand Union Canal, a historic waterway, runs through Market Harborough and terminates in Logan at its basin. Towpaths along the canal are popular walking and cycling routes for locals and visitors alike, with picturesque views of its natural setting.

Welland Park is another well-used recreational space by local communities. Located in the town centre, the park offers a range of amenities from arboretums, playing fields to play areas. There are also plenty of walking routes along River Welland.

In the north of the Neighbourhood Area is a large parkland set between the new developments at Wellington Place and Lubenham View Estates. This park will serve the growing population in this northern part of Logan which is experiencing exponential growth.

Aside from these key open spaces, there are smaller pockets of open spaces across Logan and Town Centre. These include Harborough Memorial Gardens adjacent to The Square, Logan Street Recreation Ground, Stevens Street Allotments and Market Harborough Cricket Ground. These, along with smaller pockets of play areas and amenity spaces, contribute towards the green network across Logan and Town Centre.



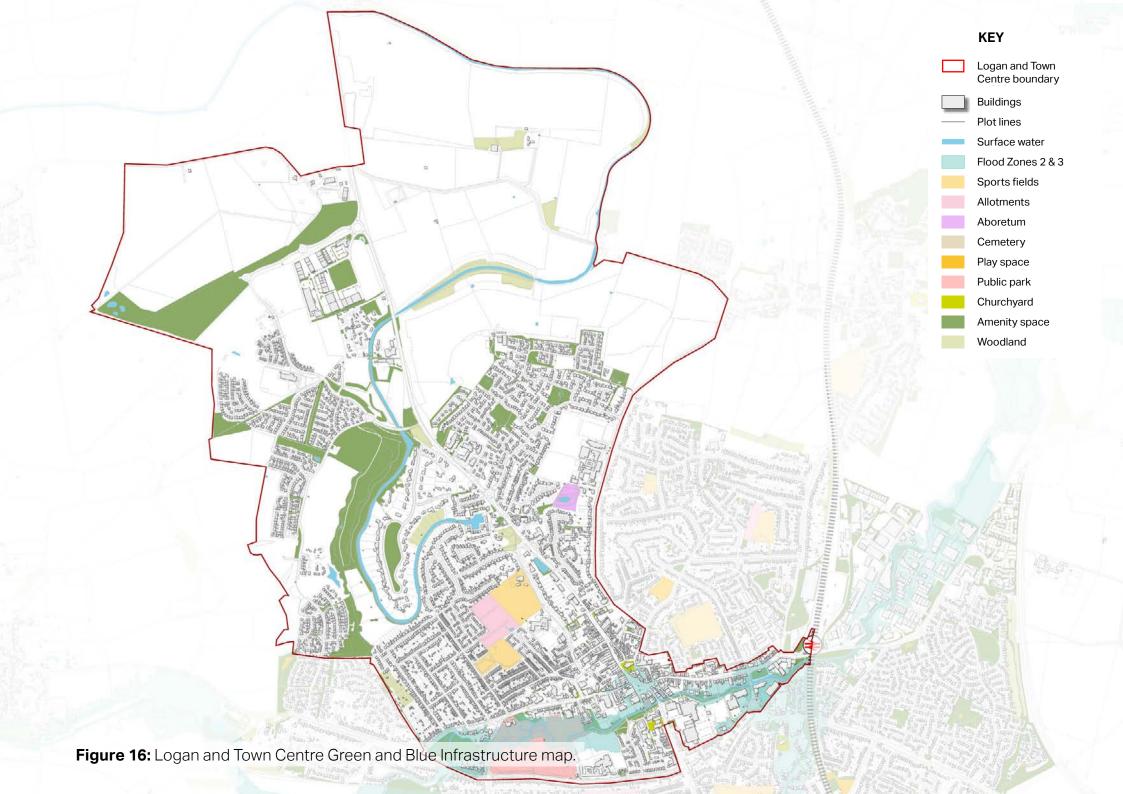
Figure 12: Grand Union Canal and Union Wharf



Figure 13: Harborough Memorial Garden



Figure 14: Welland Park



2.6 Access and movement

Most parts of Logan and Town Centre are relatively well connected, making movement efficient for residents and visitors. Logan benefits from having a varied but compact streetscape that is well structured, with a mix of linear streets and also ones that are meandering. Key thoroughfares that provide access between and across Logan and Town Centre are Coventry Road and Leicester Road. A series of local roads, such as Logan Street, Burnmill Road, Welland Park Road, Harvest Road and Alvington Way provide local access to various neighborhoods arranged on smaller winding streets and cul-de-sacs.

Key amenities are all within walking distance for those that live close to the town centre. However, newer parts of Logan to the north are rather disconnected from key amenities that are concentrated in the town centre, and are highly dependent on the private car.

The town centre is well-served by bus routes, however, bus stops are mostly located along the main routes of Coventry Road and Leicester Road beyond the town centre, making the rest of Logan less accessible via the bus. The railway station, located on the southeastern tip of the Neighbourhood Area offers direct services

to Leicester, other Midlands towns and London. However, most neighbourhoods in Logan remain disconnected from the station.

The National Cycling Network (NCN)
Route 6 is located along Logan Street,
which connects onto the Grand Union
Canal towpath to the north and through to
Welland Park to the south. The tranquil canal
towpath is a popular walking route for locals
and dog walkers, and is a defining feature
for the character of Logan and Town Centre.
Public footpaths are far and few between
across the Neighbourhood Area, with only
a few to the north which leads off from the
canal towards surrounding countryside
towards Gartree, and another to the east
towards Great Bowden.



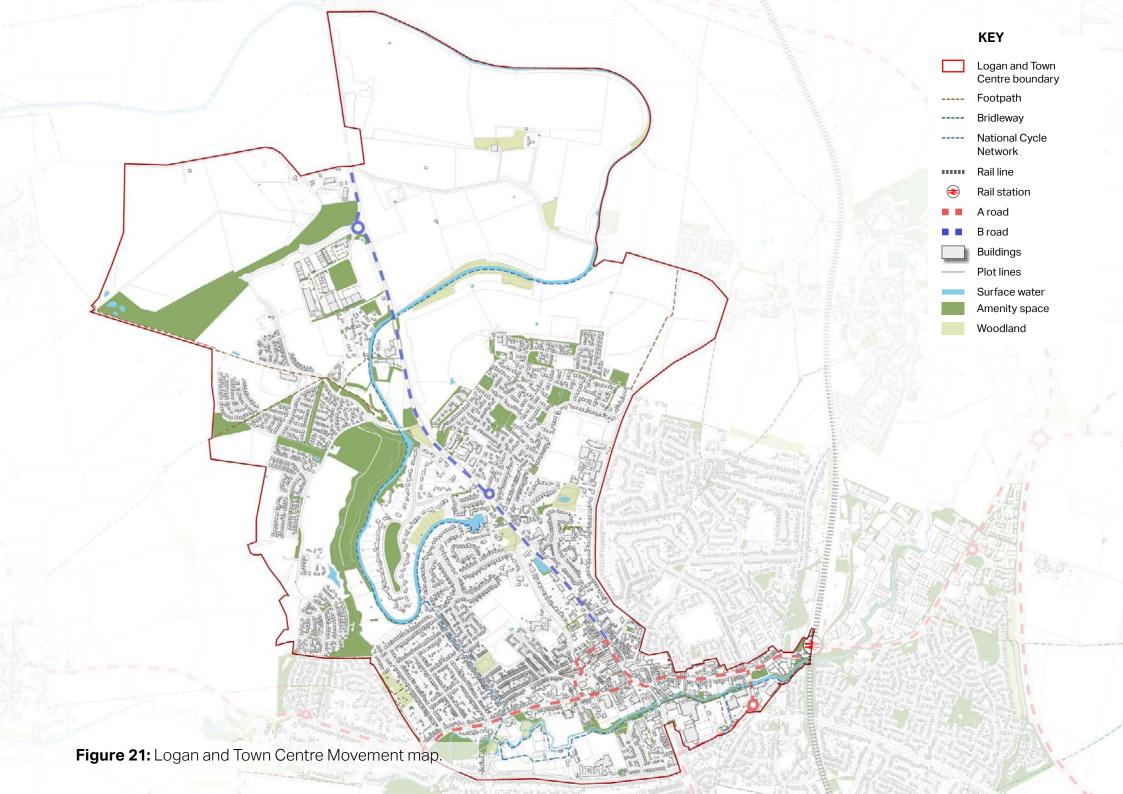
Figure 17: High Street



Figure 18: Coventry Road with cycle lanes



Figure 19: Grand Union Canal towpath





3. Market Harborough-wide design guidance

Development in the four wards of Market Harborough should demonstrate how best practice design guidance contained in national and local policy and guidance documents, including this design guide, has been considered in the layout, architectural and landscape design.

3.1 Introduction

From site visits, desktop analysis and consultations with the members from the four neighbourhood forums several common themes to be addressed have arisen. These themes are addressed by the design guidance and codes below.

Design guidance and codes in this chapter are 'good design' practice and are to be applied to the whole of Market Harborough. This will ensure that development across the town is coherent.



Design Guidance

Theme heading	Design guidance	Page number
A: Settlement pattern	A1 - Responding to context and pattern of development	25
	A2 - Infill and backland development	27
	A3 - Recognition of local vernacular and materials	28
B: Green and blue infrastructure	B1 - Open spaces, water courses and biodiversity	29
	B2 - Eco-design and sustainability (including SuDS)	31
C: Movement and connectivity	C1 - Active travel and interconnected neighbourhoods	33
	C2 - Public realm enhancements	34

Table 02: Area wide guidance table.

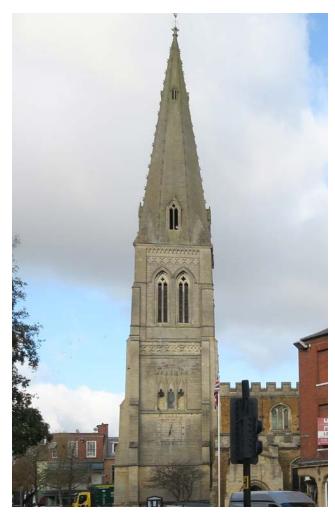


Figure 22: St Dionysius' Church, Market Harborough.



Settlement pattern

3.2 Settlement pattern

The town has developed around its central market area, which has been a focal point since the 13th century. The settlement pattern is characterised by a mix of residential, commercial, and industrial areas, with a notable concentration of development along the main roads and near the town centre.

Market Harborough's layout includes a variety of housing types, from historic buildings in the town center to modern developments on the outskirts. The town also serves as a key employment area with numerous small and medium-sized businesses.

The guidance in this section will ensure that the built form of future development is keeping with the existing context.

A1 - Responding to context and pattern of development

The pattern of development is varied throughout the town of Market Harborough. Although many areas of the town have a nucleated development pattern, more recent development such as those in Arden splay outwards from the main core. There are also areas of linear development pattern particularly in Arden, Welland and Logan.

New development proposals should consider the following principles when designing the layout of streets and plots:

- Be informed by the density and scale of development within its context;
- Respect the historic, landscape and other key features of the Neighbourhood Area;
- Maintain a positive aspect onto key spaces and features;

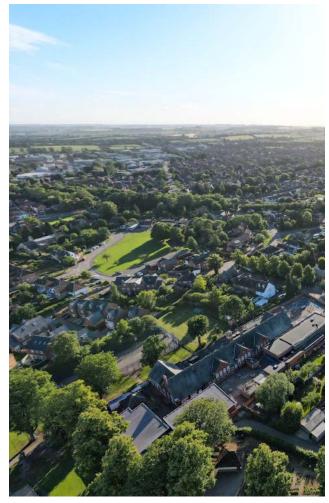


Figure 23: Drone shot of Market Harborough.

- Reinforce the linearity of the street with building frontages, where possible; and
- Avoid development with a hard edge which imposes an abrupt transition from the settlement to the surrounding countryside. Suitable boundary treatments on the periphery of the settlement may include low walls to soft landscaped edges.

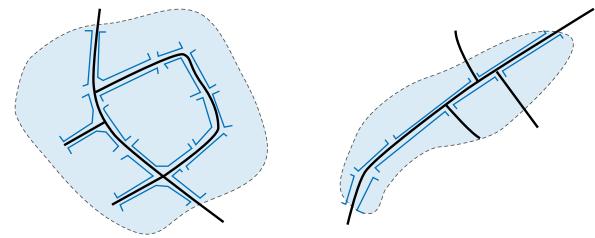


Figure 24: Diagram showing a nucleated development pattern (left) and a linear pattern development (right)



Figure 25: Nucleated pattern in Market Harborough



Figure 26: Liner pattern in Market Harborough

A2 - Infill and backland development

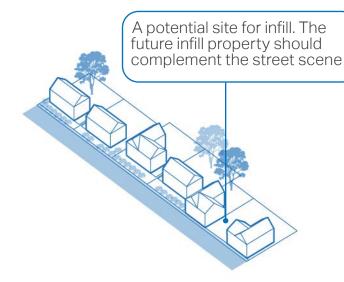
Backland development or plot infill is development on land of an existing dwelling. In the case of Market Harborough, there have been recent instances of infill/backland development. It is important that these sorts of development are respectful to the context of the area it is set, in order to preserve the identity of the town.

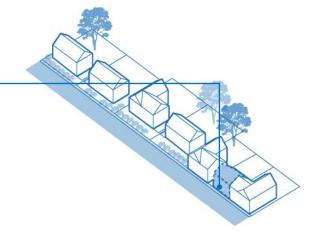
- Any new backland development should ensure that the spacing, density, scale and appearance of the development reflects its immediate context and reduces impacts to the amenity of existing properties; and
- Where a proposal involves residential development on land behind an existing frontage or placing of further dwellings behind existing dwellings within the site, the proposal should demonstrate

the privacy of existing and future residents and adequate means of access, and it should not extend beyond the limit of the settlements.

New building lines should be consistent with existing properties. Some places in the Neighbourhood Area have linear or regular meandering arrangements of buildings while others have random and irregular patterns. The infill should also reflect the surrounding context in terms of form, materials and scale

Figure 27: An indicative diagram showing a site before and after infill development with guidance attached in the annotations.

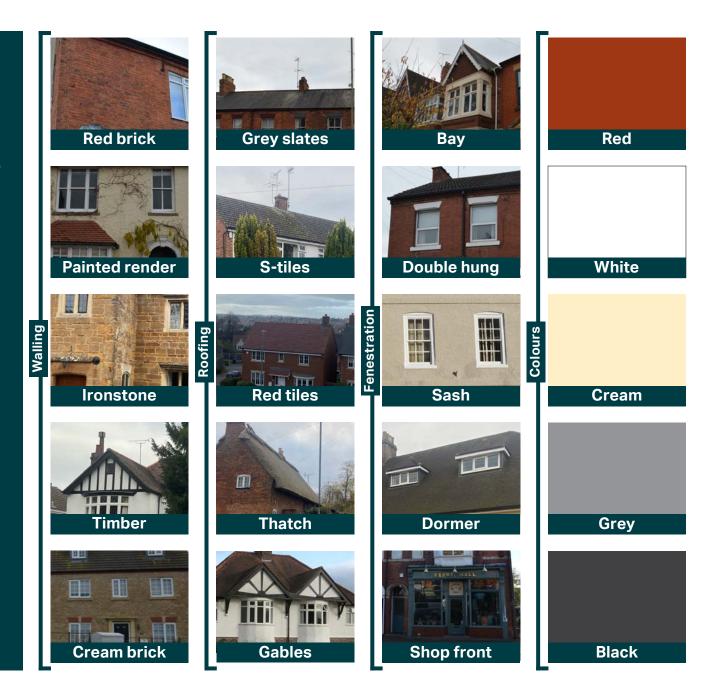




A3 - Recognition of local vernacular and materials

Future constructions must demonstrate respect for existing architectural styles and utilise materials that are considerate of those employed in nearby residences and respect the vernacular. Key materials and finishes found across the town are listed on the following page.

- New developments should draw inspiration from the high-quality local design references. It is essential for these designs to make a meaningful contribution to preserving the rural character of the town; and
- New development should ensure that it puts forward a comparable level of greenery, incorporating native and context-appropriate plant species, to establish a cohesive setting that aligns with the existing natural environment.





Green and blue infrastructure

3.3 Green and blue infrastructure

Green and blue infrastructure relates to the network of natural areas and features that provide a richness of ecological benefits, and amenity spaces. A strong green and blue infrastructure network can help improve environmental conditions and biodiversity, increase climate resilience, and benefit wellbeing.

Market Harborough is populated with a number of open spaces that serve residents. The River Welland is an important asset to the town, providing ecological richness and amenity spaces for residents to enjoy.

The guidance in this section will ensure that future development thoughtfully incorporates green and blue infrastructure in order to maintain and strengthen the existing network that permeates the village, and in order to strengthen the environmental impact of new development.

B1 - Open spaces, water courses and biodiversity

Future development must safeguard and enhance the quality of and access to open spaces, water courses and biodiversity in Market Harborough.

- Existing vegetation, mature trees and hedgerows should be preserved by incorporating them into any new landscape design and using them as landmarks where appropriate;
- Where possible, new developments must incorporate open green spaces and vegetation, enhancing the village's green feel and connection to natural areas. These can be provided as public green spaces, within front gardens and boundary treatments;
- Native tree species should be used to ensure planting is appropriate for the local character and to ensure climate resilience;

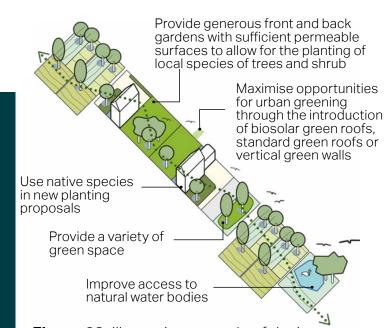


Figure 28: Illustrative example of design principles to enhance green infrastructure



Figure 29: Image along Market Harborough Arm of Grand Union Canal by Christine Johnstone

- New developments should prioritise tree planting, identify existing biodiversity corridors, and contribute to their preservation and enhancement;
- Applicants should consider how the layout can create wildlife corridors.
 For example, the layout of roads, front and back gardens, and green spaces; and
- Porous surfaces should be included at every opportunity to reduce surface water runoff and soil degradation.



Figure 30: Example of a Swift brick under an eave



Figure 32: Example of a bat box on the side of a building



Figure 31: Example of a hedgehog corridor in a garden fence

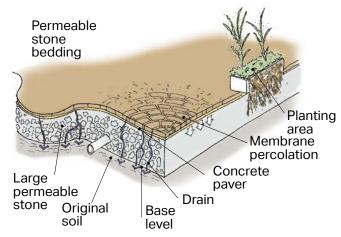


Figure 33: Section through permeable paving

B2 - Eco-design and sustainability (including SuDS)

Future development must demonstrate sustainable design in order to decrease environmental impact, both in terms of performance and how they interact with the landscape.

- The use of daylight in residential design helps improve overall health and performance, as well as provide energy savings. The orientation of buildings and roof pitches should incorporate passive solar design principles and allow for efficient solar energy collection. The following guidance should be considered when designing the aspect and orientation of any new development;
- Where possible, one of the main glazed elevations should be within 30* due south to benefit from solar heat gain. Any north-facing facades might have a similar proportion of window to wall area to minimise heat loss;

- Homes should be designed to avoid overheating through optimisation of glazed areas, natural ventilation strategies including openings, longer roof overhangs, deep window reveals and external louvres/shutters to provide shading in hotter months;
- North-facing single aspect units should be avoided or mitigated with

- the use of reflective light or roof windows; and
- All proposals must incorporate sustainable drainage systems (SuDS).



Figure 34: Illustrative graph showing solar orientation of a room against the annual heating demand.



Movement and Connectivity

3.4 Movement and connectivity

The A4304, running east to west, along with the B6047 and A508, running north to south, sections Market Harborough into four quadrants. These roads make Market Harborough well-connected and provide strong routes to surrounding areas and also major national roads, such as the M1 and M6. A network of B roads and minor roads form connections within the built up and residential areas of Market Harborough, with a mix of both meandering roads in the newer areas and much more linear forms in the terraced historic sections.

There are many bus routes that serve Market Harborough and provide direct connections to surrounding villages and parishes, also extending out to Northampton and Leicester.

The train station is situated at the heart of the town, within a 10-minute walk from the town centre, with bus routes that stop

directly outside, making it accessible from farther afield too. The station services East Midlands Railway (EMR), providing direct access to London St Pancras and Nottingham, both of which take an hour or less.

Market Harborough benefits from a number of Public Rights of Way (PRoW) that connect the more urban areas to the surrounding countryside. Brampton Valley Way, which is a 14-mile trail built on the way of the former Northampton to Market Harborough railway line, runs into the south of the town and is frequently used by pedestrians, cyclists and also horse-riders.

The National Cycle Network (NCN) Route 64 begins in the center of the town and runs north to Lincoln, along with NCN 6, which is one of Britain's longest cycle routes, which runs south to northeast of the NA, and goes from London up to the Lake District.



Figure 35: Front view of Market Harborough train station



Figure 36: Parked cars on a densely packed terraced street.

C1 - Active travel and interconnected neighborhood

- New streets must be designed as a 'space' to be used by all. Existing streets should be retrofitted for the same purpose and to discourage speeding;
- Development should design internal streets and paths that are well-connected and direct, responding to desire lines;
- Establishing a pedestrian network between existing and new developments is a key principle for encouraging active travel and must be considered;
- Pedestrian and cycle networks within residential communities should enable natural surveillance and offer unrestricted views to make people feel safer;
- A green network should be created and enhanced from new and existing pedestrian cycle links to

further encourage usage on a daily basis; and

 Barriers to vehicle movement, gates to new development, or footpaths should all be accessible to other users.

Traffic calming

Traffic calming measures can be introduced to existing streets to shift the hierarchy of movement from motor vehicles to prioritise pedestrians or cyclists. The following measures can be used independently or in combination with one another:

- Junction design;
- Raised junctions and entry;
- · Continuous footpaths; and
- Kerb extensions and build outs.



Figure 37: Footpath integrated within residential development offering alternative walking and cycling routes to people, Great Kneighton, Cambridge.



Figure 38: Footpath linking the north of Arden with Great Bowden through the area of separation

C2 - Public realm enhancements

High-quality, well connected public spaces are essential for towns. They create informal meeting places, offer a place to rest and can even provide shelter. The public realm should be coordinated and strengthen local distinctiveness and uniqueness, making it a more legible and user friendly environment. This can be done via:

- Street furniture these should be added in appropriate locations to provide people with places to stop and linger, distinctive street furniture can improve legibility and wayfinding whilst enhancing character of a place. Choice of materials should be of high quality that complements the surrounding area as well.
- Pop-up installations to reclaim streets combination of planters

and street furniture can be used to activate the whole or parts of the street (e.g. parking spaces), allowing such spaces to be temporarily reclaimed to form a pop-up space for more informal social and lots of community engagement.



Figure 39: Central green space overlooked by neighbouring development, Middlebrook Green



Figure 40: Properties overlooking a public open space in Poundbury



Figure 41: Residential street with grass verges and trees, The Headlands



4. Townscape character assessment

This section identifies and explains the different area types in Market Harborough. Places have a clear and strong identity and character. They are a combination of their physical form, their activities and their meaning to people.

4.1 Assets and challenges

The analysis highlights several important assets and challenges that are integral to the unique character of Logan and Town Centre.

These assets collectively paint a picture of some of the neighbourhood area's most valuable features and areas for improvement. Preserving the assets and tackling the challenges is key in maintaining Logan and Town Centre's distinctiveness and ensuring it remains a desirable place for residents, businesses and visitors alike.

By safeguarding the assets and making sure that current constraints are considered, the essence of Logan and Town Centre can be protected, fostering and environment that continues to attract people and support a thriving community. The preservation of these assets will help ensure that the area retains its identity and continues to flourish.

Rich heritage assets

- There are number of well-preserved heritage assets located in the Town Centre and the Union Wharf area that contribute positively to area's character.
- Some of these have also been successfully retrofitted and brought into active use (e.g. the Symington Building).



Figure 42: Symington Building - an iconic Victorian corset factory which has been retrofitted and now host the Harboorugh District Council offices, library and other civic uses.

Connectivity

- Recent developments in the northern parts of Logan are disconnected from the town centre, where a majority of amenities are located.
- The cul-de-sac street pattern dominates recent developments in Logan, leading to limited permeability around the neighbourhood.



Figure 43: Annotation showing cul-de-sac street patterns in newer parts of Logan with limited permeability.



Active travel infrastructure

- Cycling infrastructure is available, albeit limited, across Logan and Town Centre - including the NCN Route 6, and the town's only stretch of segregated cycleway along Coventry Road.
- There is generally a good quality of pavements across the Town Centre, however, pavements tends to be narrower in older parts of Logan which can impact on pedestrian safety.



Figure 44: Segregated cycleway along Coventry Road.



Inappropriate parking arrangements

• Due to the prevalence of terrace housing typology across Logan, there is a lack of on-plot parking available. Many residents and visitors resolve to on-street parking and at times parking on pavements. This creates a cluttered street scene that can be dangerous for both drivers and pedestrians. In some cases, inappropriately parked cars could also block cycle lanes.



Figure 45: Parked cars mounting pavements close to street junctions, resulting in a dangerous and cluttered streetscene.



Green and blue infrastructure network

- Logan and Town Centre boasts an extensive green and blue infrastructure network, comprising of a number of parks of varying sizes, River Welland and the Grand Union Canal - contributing to local biodiversity and recreation.
- The towpath along Grand Union Canal offers a car-free walking and cycling route across the Neighbourhood Area.



Figure 46: Towpath running along the Union Canal is a popular recreational route for locals and dog-walkers.

4.2 Understanding place

The diagram opposite shows how these factors come together to create a successful place. The following character analysis was developed by creating a holistic picture of Market Harborough.

All new development must undertake its own comprehensive analysis of place to understand a proposal's broader context and establish aspirations and place-specific responses to the location, siting and design of new development.

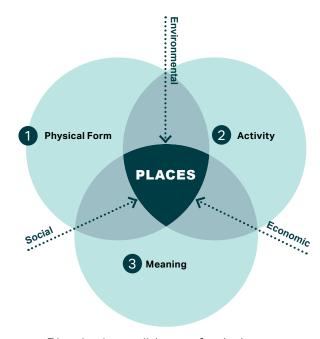
A primary purpose of this design document is to help generate sensitive and characterful design responses to existing settlements and their landscape settings.

This townscape character assessment helps understand both the landscape setting and the detailed pattern of settlement growth that underpins the variety of character features across Market Harborough.

This analysis has been cross-checked on site as part of this study with a walking tour and photographic study guided by local residents.

Each proposal may require slightly different design detail responses depending on its specific local context within Market Harborough.

Alternatively, Market Harborough may continue to acquire new layers with design approaches and concepts that are innovative and look to meet the future challenges of sustainability and biodiversity net gain. However, these responses must still seek to tie in with the landscape and townscape appeal that help give Market Harborough as a whole and the different wards their distinctive character.



- Physical conditions of existing built development including layout, form, scale, appearance, landscape character, waterways and flood risk.
- 2 Use, vitality and diversity, including community facilities and local services.
- How a place is perceived, including local heritage, views inwards and outwards and social histories.

Figure 47: Exploring the features which come together to create a successful place.

4.3 Identifying Market Harborough's Area Types

This design code reflects the varied nature and individual character of Market Harborough.

It has been divided into distinct area types, representing areas of Market Harborough with similar characteristics. The area types provide a basis for setting consistent parameters within this design code.

A thorough analysis of area types has been informed by site visits, mapping, analysis and insights from local communities. The identification process considers both the existing character of the area and future development. These area types are categorised based on overall similarities in their attributes, facilitating the creation of consistent design codes and guidance in Chapter 5. While rationalising area types, occasional anomalies arise.

However, considering factors like boundaries, street patterns, and scale, the overarching consistencies become apparent. The ten identified area types are illustrated on the map on Figure 48:

- Town Centre
- Historic Settlement
- Victorian and Edwardian terraces
- Linear suburban development
- Inter-war suburbs
- Mid 20th century suburbs
- Late 20th century/Early 21st century development
- Industrial/employment
- The Woodlands
- Non-traditional residential

These area types are dynamic and do not adhere strictly to defined boundaries, especially at their intersections or interfaces with areas outside the neighbourhood boundary. The focus of this study is on the diverse qualities inherent to each

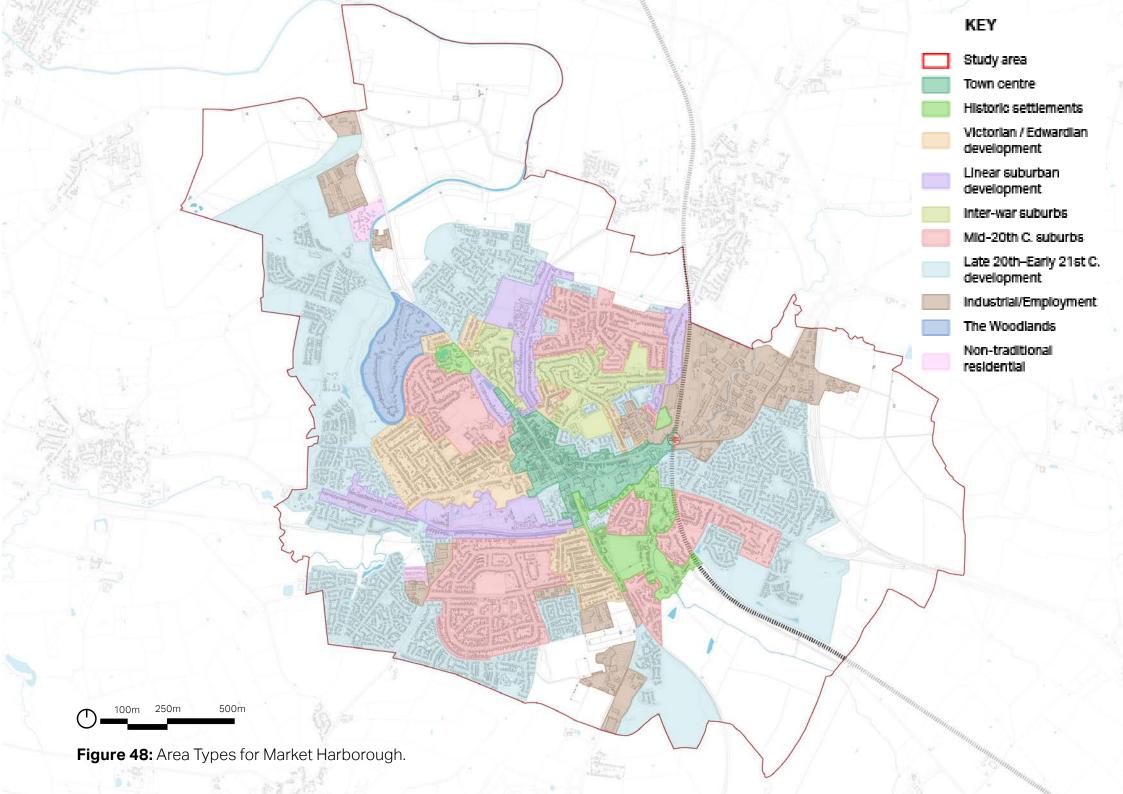
area, rather than the specific boundaries assigned to them.

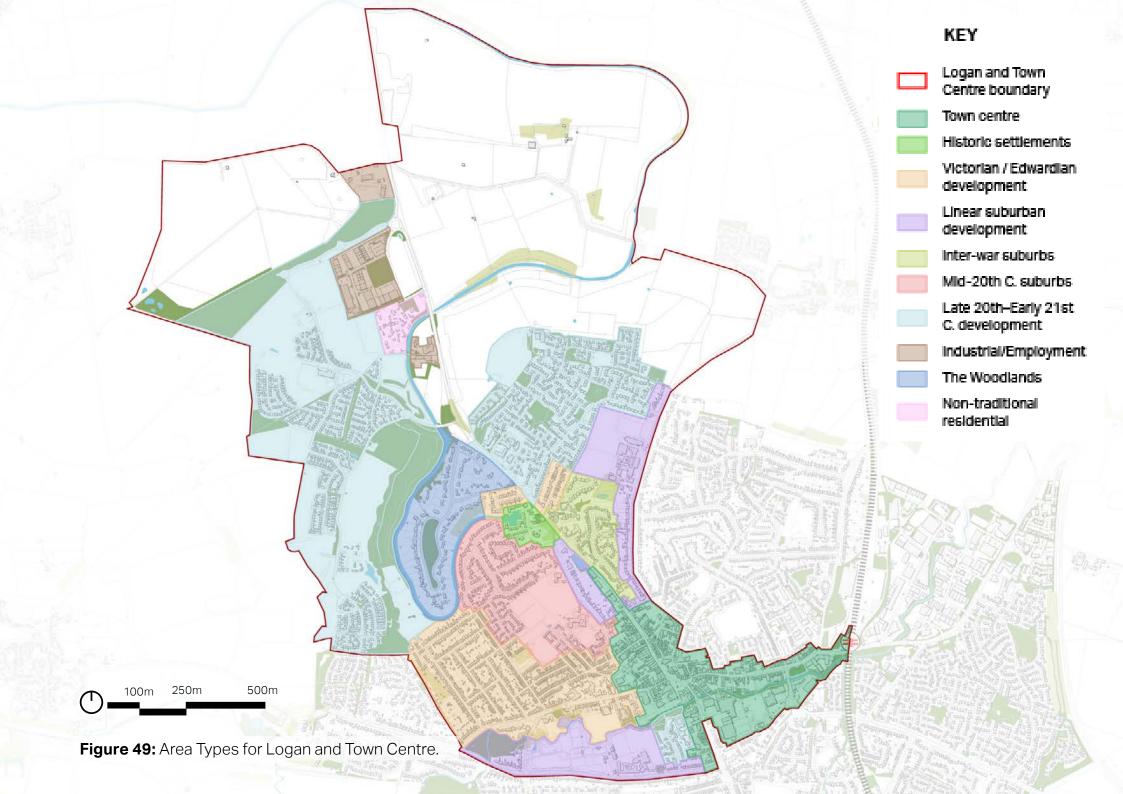
The area types allow for the development of guidelines that respect the existing character while accommodating future growth and changes.

The following attributes contribute to the character which forms the area types:

- Connections
- Urban form
- Block and plot sizes
- Boundary treatments and setbacks
- Building size, scale and type
- Green and blue infrastructure
- Public realm/open space

These attributes are analysised for the area types that are within Logan in this document.



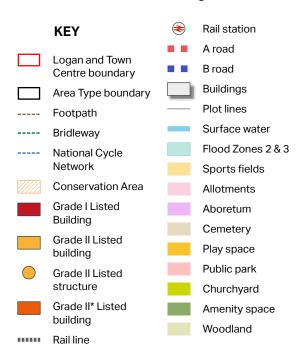




Town Centre

4.4 Town Centre

The Town Centre area type is centred around the High Street, The Square, St. Mary's Road and a series of side streets that feeds off of these main routes. The Town Centre serves as a key retail and activity hub for the town. It is also the historic core of Market Harborough with much of the area being designated as a Conservation Area with a number listed buildings.



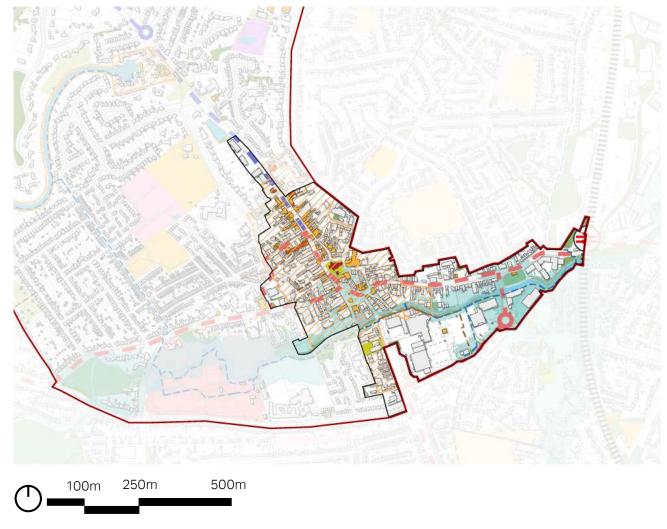


Figure 50: Map illustrating key characteristics of the Town centre area type.

Topic	Sub-topic	Written analysis
Connections	-	The Town Centre's street pattern is primarily centred around the four radial routes of the High Street (A4304), St Mary's Street, Coventry Road and Northampton Road - characterised by single carraigeway traffic and generally good quality pavements. These linear routes converge at The Square, which is a pedestrianised street at the heart of the Town Centre. Pavement build outs and kerb extensions have been implemented at junctions and intersections at The Square to ensure pedestrian safety. A series of narrower and winding side streets, such as Church Square, Roman Way and School Lane, feed off from the High Street to the east and west, serving as gateways into and out of the Town Centre. Narrow cycle lanes are available along Northampton Road and the High Street.
	Urban form	Much of the Town Centre is designated as a Conservation Area with many of its original medieval buildings now replaced by Georgian/Victorian ones - many of these are Grade II listed. The continuous frontages that these terraced buildings offer result in a tight-knit pattern of development. Some of the long narrow medieval burgage plots are still evident behind the High Street and Church Street - with long passageways leading into courtyards with retail and offices, such as Bennet's Place Courtyard, Corts Yard and Talbot Yard.
Duilt form	Block and plot sizes	Block and plot sizes are varied across the Town Centre. Whilst some of the long narrow burgage plots are still present on the High Street, many of these have been amalgamated to form larger plots to serve different types of modern day land uses - such as the parade of shops and services on No.1-4 High Street. The southeastern side of the Town Centre is characterised by a series of "big-box" retails comprising a number of supermarkets and retailers.
Built form	Boundary treatments and setbacks	There is little to no boundary treatment for developments within the Town Centre. Most buildings have little set back from the street within the Town Centre which results in a heightened sense of enclosure along these routes. Where pavements have been extended along the High Street, or around the pedestrianised The Square, there is a larger degree of setback from the streets - creating a more spacious and pedestrian friendly environment.
	Building size, scale, and type	There is generally fine grain pattern of development with subtle variation in building sizes/scale due to the prevailing Victorian and Georgian style of development. Building heights range between 2-4 storeys high, many of these serve as retail, restaurants/ cafes with some residential units above shops. Examples of larger building scales include the Symington Building on Adam and Eve Street, a past Victorian corset warehouse which now houses the Harborough District Council offices, the town library and Harborough Museum.

Topic	Sub-topic	Written analysis
Nature	Green and blue infrastructure	River Welland flows east-west across the Town Centre and is the main water feature across Market Harborough, which is bordered by treelined footpaths. The immediate vicinity to River Welland is subjected to some flood risk. Due to the built up nature of the town centre there is limited green infrastructure aside from street trees and occasional landscaping.
	Public realm/ open space	There is limited open spaces in the Town Centre, aside from the Memorial Gardens which can be accessed via the southern edge of The Square. Street trees and planters are present around The Square and along the High Street along with street furniture - such as benches, bus shelters, bins and cycle stands. However, there are far fewer street trees and landscaping to the north of High Street. A variety of paving material features across the Town Centre which helps to demarcates between different uses (e.g. road surfaces, pavements or parking bays). These range from concrete or granite sett to brick paving Yorkstone - concrete tactile paving has been integrated at pedestrian crossings to enhance accessibility.

Table 03: Town Centre area type description.



Figure 51: View of The Square and High Street from St Mary's Road.



Figure 52: St Dionysius Church (left), The Old Grammar School (right) on Church Square, with the Symington Building in the background.



Figure 53: Mix of Georgian and Victorian frontages with varying styles provide continuous and active ground floor frontages along High Street.



Historic Settlement

4.5 Historic Settlement

Union Wharf, located to the north of the Neighbourhood Area is set at the basin of the Grand Union Canal's Leicester Line. This area has rich industrial heritage associated to the distribution of goods along inland waterways across the Midlands during the C19th. Today, Union Wharf remains as a vibrant waterside local hub with a mix of apartments, independent retailers and restaurants.



Rail line

.....



Figure 54: Map illustrating key characteristics of the Historic settlements area type.

Topic	Sub-topic	Written analysis
Connections	-	Union Wharf is effectively a waterfront cul-de-sac wrapped around the Grand Union Canal basin. The area can be accessed via Leicester Road via a gently meandering access route, or on foot along the towpath which follows the winding canal northwards. There is a cut-through along the towpath offering onward connections towards New Harborough via Logan Crescent.
	Urban form	The area is primarily defined by its location along the Grand Union Canal, which influences its landscape setting, tranquillity and architectural style. Due to the historical use of the buildings, the scale and size of them has allowed for successful conversion to mixed use apartments.
Built form	Block and plot sizes	Around Union Wharf, the plots are smaller and more regular due to the uniform nature of the historical buildings. Blocks of apartments in the wharf have larger massing and building footprints that mimic the style of warehouses that once occupied the wharf. The apartment block on the west has deeper setback and is elevated from the edge of the water, preserving privacy of residents.
Built form	Boundary treatments and setbacks	Boundary treatments vary, but are predominantly dense green bushes of differing variety and height, minimising the homogeneous feel to the area, and adding visual interest. There is little setback with most building fronting relatively close to the water.
	Building size, scale, and type	Building size is relatively uniform where historic warehouses have been retained converted into apartments, and various public facilities - preserving the Wharf's original industrial scale and appearance. Buildings vary from two to four storeys but are all built from red brick with slate or clay tile roofs.
Nature	Green and blue infrastructure	The Grand Union Canal is the central water feature to this area. The wharf is equipped with a marina, allowing for private moorings and day hire of narrow boats for navigation along the canal. The access routes have large green verges either side and there are varying types of boundary treatments, adding the green characteristic of the area. The green infrastructure varies from small potted plants and bushes, to large, dense hedgerows, creating visual interest.
	Public realm/ open space	Green and waterfront spaces are both provided in this area for a range of leisure and recreational uses. The towpaths provide a safe, car-free alternative access route for pedestrians and cyclists, contributing towards the promotion of active travel. The Waterfront bar and restaurant attracts a social crowd in both the day and night, especially on weekend - making the area s bustling local activity hub.

Table 04: Historic Settlement area type description.



Figure 55: Converted warehouse which serves as a restaurant with outdoor seating pods fit for wet weather, fronting directly onto the wharf and marina.



Figure 57: Apartment block with balconies overlooking the wharf, the use of red brick remain in-keeping with the surrounding existing warehouse buildings. © *Copyright Stephen McKay (Source: Geograph)*



Figure 59: Towpath along the Grand Union Canal provide a car-free and tranquil connection to Union Wharf from other parts of Market Harborough.



Figure 56: On-street parking for residents and visitors, these areas can become very busy during weekends with streets becoming cluttered with parked cars.



Figure 58: Apartment block built to mimic the style, scale and massing of existing warehouses - characterised by large windows, the use of red brick, and boundary treatment in the form of shrubs and planting.



Figure 60: Due to the elevated topography of the wharf, long views out towards Market Harborough are visible between gaps of buildings.



Victorian and Edwardian Development

4.6 Victorian and Edwardian Development

Large pockets of Victorian and Edwardian terraces can be found predominantly in the New Harborough area to the west of the town centre, which were built as extensions of the historic town centre smaller pockets can also be found to the north. Development in these areas are characterised by rectilinear layouts, and consistent building lines.



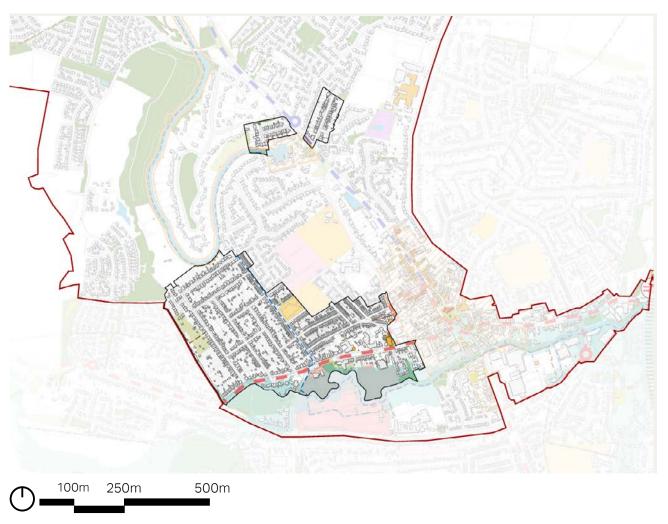


Figure 61: Map illustrating key characteristics of the Victorian and Edwardian area type.

Topic	Sub-topic	Written analysis
Connections	-	Streets patterns in the New Harborough area follows a grid pattern, they tend to be narrow and offer little room for on-street parking. Pavements are present but are also narrow in width. Victoria Avenue and Hillcrest Avenue to the north follow a cul-de-sac street pattern and these streets remain narrow.
	Urban form	The urban form of this area type is largely defined by a rectilinear shape given rise from the terrace typology. Building lines and styles of housing are highly consistent to provide continuous frontages along the quiet residential streets. Houses along Coventry Road demonstrates a linear pattern of development following the road.
Puilt form	Block and plot sizes	This area type is generally characterised by a fine and tight-knit urban grain as a result of the terrace typology. Plot sizes tends to be narrow, offering moderately sized back gardens and shallow front gardens. Due to a lack of appropriate on-street parking arrangements, many residents resolve to using their front garden for parking. Larger detached and semi-detached houses can be found on Coventry Road which are set on large plots with long back gardens and generous front gardens.
Built form	Boundary treatments and setbacks	There is generally little setback which is largely uniform across the area type. There are more variation observed on Victoria Avenue and Coventry Road. Boundary treatments are varied but generally hardscaped in New Harborough - including low brick walls, metal railing and wooden fencing. Natural boundary treatments, such as hedgerows, trees and landscaping, are rare in New Harborough and tend to feature more along Coventry Road, Victoria Avenue and Hillcrest Avenue where gardens tend to be more generous.
	Building size, scale, and type	Buildings are predominantly 2-storeys and follow the terrace or semi-detached typology. Consequently building lines and scales are largely uniform. Examples of units with larger scale are found on Coventry Road.
Nature	Green and blue infrastructure	Street trees, hedges and other natural boundary treatment contributes towards the green infrastructure of the area type. Logan Street Recreation Ground in the New Harborough area, as well as Welland Park to the south where River Welland runs through also form integral parts of the area's green and blue network.
	Public realm/ open space	Welland Park and Logan Street Recreation Ground are the key open spaces for the area type. The streetscape of the area type tends to be hardscaped and cluttered by on street parking.

Table 05: Victorian and Edwardian Development area type description.



Figure 62: A strong sense of linearity and enclosure are achieved by the terrace typology and a highly uniform use of red brick across most properties.



Figure 64: Red brick terrace housing setback from the Coventry Road, with onstreet parking.



Figure 66: Large Victorian detached house on Coventry Road, with red brick facades, decorative stone porch, and stone-framed bow windows. Pitched roofs are decorated with white timber barge boards.



Figure 63: Rows of red brick terraces in the New Harborough area, with mock tudor timber frame features and stone lintels above windows. Properties are aligned relatively close to the pavement.



Figure 65: Victorian red brick terraces on Logan Street, featuring white timber frame bow windows. Inappropriate on-street parking and paving mounting by vehicles are commonly observed.



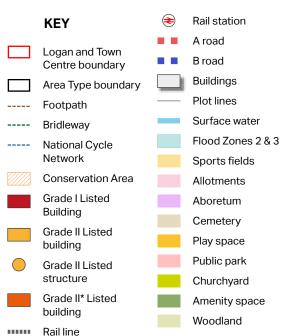
Figure 67: Terraces framing the street corner between Logan Street and Northleigh Grove, maintaining a strong sense of enclosure around the street corner.



Linear Suburban Development

4.7 Linear Suburban Development

Linear suburban development is typical of early 20th century urban development, where development occurs in a linear fashion along a road. In Logan, this type of development are found along Burnmill Road and Fairfield Road. This area type is characterisd by long continuous stretches of housing, with little through permeability.



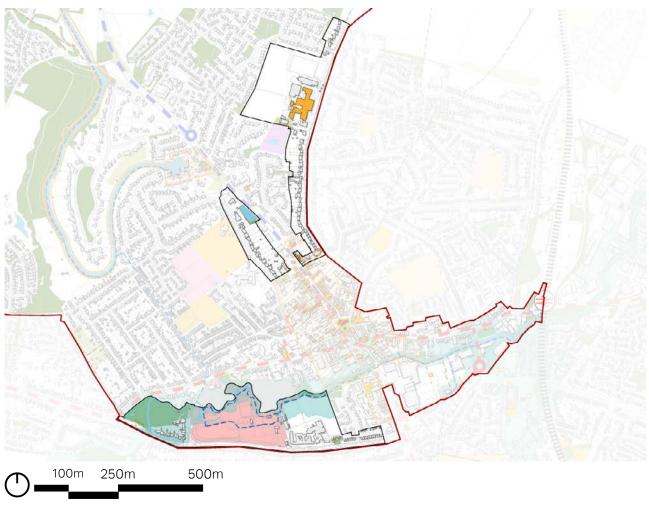


Figure 68: Map illustrating key characteristics of the Linear suburban development area type.

Topic	Sub-topic	Written analysis
Connections	-	There is little connectivity and permeability in this area type, as a result of long continuous rows of housing along wide streets without any breaks from side streets or footpaths. This is especially evident along Burnmill Road, the longer of the two roads in the area type, and a key north-south through-route in Logan.
	Urban form	Urban form is varied in this area type due to different levels of setback and highly varied styles of housing. However, a strong sense of linearity is maintained as a result of consistent building lines which closely follow the roads. There is a gentle incline northwards along the Burnmill Road, allowing for views between gaps of buildings.
Built form	Block and plot sizes	Block and plot sizes are generous across the area type, providing generous front and back gardens, as well as on-plot parking with many properties also having garages. Plot sizes are more modest on Welland Park Road, compared to those of Burnmill Road and Fairfield, consequently front and back gardens are also smaller for these properties.
	Boundary treatments and setbacks	Setbacks for the area type is varied as a whole, with deeper setbacks observed on Burnmill Road where front gardens tend to be the largest out of the three areas. Boundary treatments often include hedges, landscaping, grass verges and low brick walls. Impervious paving feature frequently in front gardens within this area type. There are often wide grass verges set between pavements and front gardens.
	Building size, scale, and type	Properties tend to be 2-storey semi-detached and detached houses, whilst some bungalows can also be found along Fairfield Road. Properties along Welland Park Road are more modest in size and scale compared to those of Burnmill Road and Fairfield Road.
	Green and blue infra- structure	All areas of this area type tends to feature ample of street trees with large canopies and wide grass verges along pavements. Welland Park and River Welland that flows through the park also serve as key green and blue infrastructure for Logan.
Nature	Public realm/ open space	Treelined streets and landscaped front gardens provide a generally leafy feel to the streetscapes of this area type. Open spaces are sparse in the Burnmill Road area, but properties along Fairfield Road and Welland Park Road benefit from being in close proximity to the Market Harborough Squash and Cricket Club and Welland Park respectively.

Table 06: Linear Suburban Development area type description.



Figure 69: Buildings are large and set on generous plots with deep setback, resulting in large front gardens and back gardens, Burnmill Road.



Figure 71: Property with solar panels installed on roof, long hedgerow set at the boundary to delineate between public and private areas, Burnmill Road.



Figure 72: Tall hedgerow and trees serving as an effective boundary treatment, allowing property to maintain privacy, Burnmill Road.



Figure 73: Building styles and use of material is highly varied in this area, but white render, light coloured render and red brick are commonly featured materials., Burnmill Road.



Figure 74: Fairfield Road is characterised by ample of street trees and grass verges, creating a leafy atmosphere. Properties have deep setbacks from the road. © Copyright Robin Webster (Source: Geograph)

form and roof profile, doorway is framed by a red brick archway paired with black bow casement windows, Burnmill Road.

Figure 70: Property with an interesting



Inter-war Suburbs

4.8 Inter-war Suburbs

Inter-war suburbs are characterised by detached and semi-detached houses with some short terraces with front and back gardens. The layouts often reflect the influence of the Garden Village movement. In Logan, inter-war suburbs are mainly found along Edward Road, Park Drive, Holly Close and parts of Leicester Road.



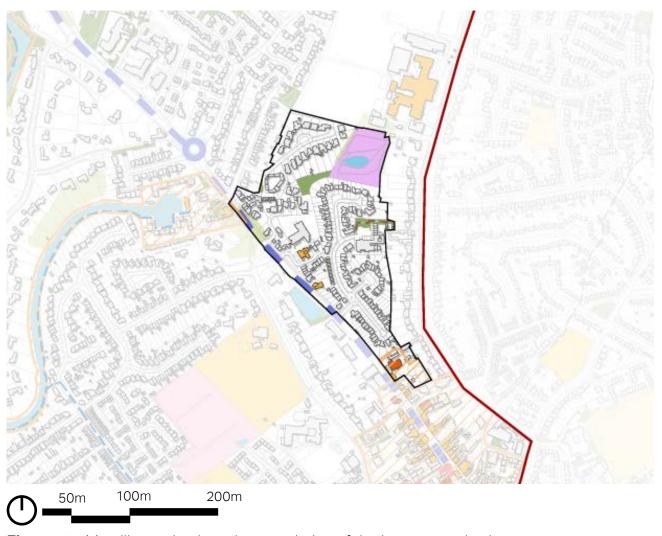


Figure 75: Map illustrating key characteristics of the Inter-war suburbs area type.

Topic	Sub-topic	Written analysis
Connections	-	Properties tends to be arranged on gently meandering streets and cul-de-sacs. There is generally an absence of through-routes which helps preserve the tranquil residential character. Park Mews is an exception to that, offering pedestrian connections between the end of Park Drive and Burnmill Road.
	Urban form	There is also a gentle incline up Park Drive and Edward Road, allowing for views out to surrounding neighbourhoods between gaps of buildings. The cul-de-sac arrangement create less uniform building lines, together with a variety of housing styles and use of material provide dynamic streetscapes across the area type. There is however more uniformity in style for properties along Birtley Coppice and Poplars Court, which are more recent infill developments.
	Block and plot sizes	Plot sizes tends to be moderately sized, providing well-sized back gardens but shallower front gardens. Park House apartments on Park Drive and apartments at Poplars Close are the larger blocks within the area type, with the former being a property of the Symington Family.
Built form	Boundary treatments and setbacks	Houses are typically set behind a front garden, delineated by a variety of boundary treatments mostly comprising of hedges, shrubs and other ranges of natural landscaping - which are especially effective to serve as buffer along the main road of Leicester Road with fast moving traffic. Some boundaries and gardens have been altered to accommodate on-plot parking. Gentle variations in setbacks are introduced in places for visual interest or to create small amenity green spaces such as those on Edward Road. Properties along Birtley Coppice and Poplars Court tends to be dominated by hardscaped boundary treatments and impervious surfaces.
	Building size, scale, and type	A majority of the properties are bungalows or 2-storeys converted bungalows, with the exceptions for apartments in Park House, Poplar Court and on Leicester Road - which are of 2-4 storeys high.
Nature	Green and blue infrastructure	Much of the green infrastructure of the area can be attributed to the ample of natural landscaping in gardens, along with treelined and grass verge lined streets
	Public realm/ open space	Due to a majority of housing being bungalows in this area type, rooflines are largely set below treelines. There is generally a more spacious and leafy feel to these neighbourhoods.

Table 07: Inter-war suburbs area type description.



Figure 76: Park House apartments - built with red brick, stone quoins and stone lintels above windows, the property has past associations with the Symington Family.



Figure 78: Bungalows with large street facing windows and exposed chimney breast, built with a mix of brick and white render, Park Drive.



Figure 80: Red brick and white rendered bungalows, with slight setback from the street, boundaries are delineated by low brick walls and wooden fence.



Figure 77: Due to the low lying nature of bungalows, many of the properties have rooflines that are set below treelines - allowing them to form a scenic backdrop.



Figure 79: Bay windows and casement windows are common styles of fenestration for the area.



Figure 81: Detached house with grey tiled mansard roof, red brick facade paired with bay casement windows, the unique style of the property and its setting at the street corner makes it a key landmark for the area. © Copyright Andrew Abbott (Source: Geograph)



Mid-20th Century Suburbs

4.9 Mid-20th Century Suburbs

As is common in a lot of settlements in the UK, Market Harborough underwent significant housing development in the mid-20th century. These are located to the north of the town centre in Logan, and are suburban housing estates set on gently meandering streets and cul-de-sacs with quiet residential streetscapes.



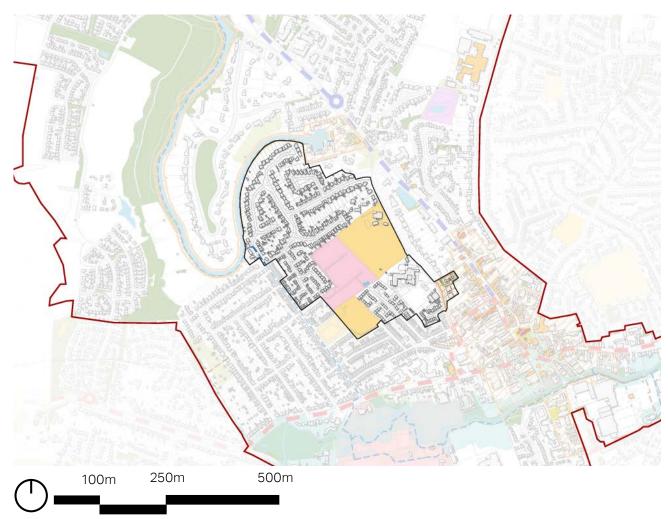


Figure 82: Map illustrating key characteristics of the Late 20th-Early 21st C. development area type.

Topic	Sub-topic	Written analysis
Connections	-	Developments are articulated along winding roads, with most of these being cul-de-sacs and very few through-routes. Due to this arrangement streets remain open and tranquil, which helps preserve their residential character. A few 'cut throughs' provide additional pedestrian connectivity, including a jitty way at Logan Crescent which connects onto the scenic towpath along the Grand Union Canal leading to the north or towards Union Wharf.
	Urban form	This area is characterised by post-war housing arranged along gently meandering crescents and shorter closes. Building lines follow meandering streets which results in subtle variation in angles of orientation, enriching visual interest of the streetscape in the area. Topography rises gently along Northleigh Grove and Logan Crescent, properties along both streets back onto the canal and are set against a backdrop of mature trees along the canal.
Built form	Block and plot sizes	Blocks and plot sizes are typically generous, in line with mid-20th century size standards, with most homes featuring a front garden and front-of-plot or side-of-plot parking. Blocks and plot sizes for infill developments in the late 90s/early 2000s on Austins Close and Norbury Close are relatively smaller compared to other parts of the area - resulting in a tighter-knit development pattern.
	Boundary treatments and setbacks	Boundary treatments generally include low brick walls, wooden fences, often with hedges or informal plantings, and grass verges/clipped lawns. In some properties, the entire front garden is paved and occupied by on-plot vehicle parking - this is particularly prevalent in the more recently developed Austin Close and Northleigh Close.
	Building size, scale, and type	There is a mix of bungalows, 2-storey semi-detached and detached houses, and extended bungalows. There is also a minority of short one- and two-storey terraces dispersed around the area. Overall, the variety in housing style does not detract from the cohesiveness of the streetscape.
Nature	Green and blue infrastructure	Green infrastructure in this area type through the use of natural boundary treatment - such as hedges, street trees and wide grass verges along streets and pavements. Additionally, Stevens Street allotment sites - one of the largest allotments in Market Harborough, together with various school playing fields, contribute significantly towards the green infrastructure and local biodiversity of the area.
	Public realm/ open space	Market Harborough Cricket and Squash Club and Logan Street Recreation Ground are the key open spaces provision to the area. The openness and leafy nature of streets create pleasant streetscapes in the area.

Table 08: Mid-20th century suburbs area type description



Figure 83: The area is defined by curvilinear cul-de-sacs, properties have subtle variations in setback and orientation - adding visual interest to the streetscene.



Figure 85: Continuous hedgerow framing the boundary of a corner unit at Logan Crescent.



Figure 87: Many of the properties in this area are extended bungalows, featuring well-proportioned shed dormers.



Figure 84: Property with garage below the house, and a driveway with permeable gravel paving.



Figure 86: Footpath leading to the Grand Union canal towpath from Logan Crescent.



Figure 88: View towards other parts of Logan are visible from gaps between buildings due to the elevated topography in this area.



Late 20th Century/Early 21st Century Development

4.10 Late 20th Century/Early 21st Century Development

Since the late 20th century, Market Harborough has experienced several housing developments. Development of this time period makes up a large proportion of the northern parts of Logan. These are also parts of the town that have seen significant growth in the last 10 years. These developments include many different typologies with some mock Victorian terraces, semi-detached and detached houses.





Figure 89: Map illustrating key characteristics of the Late 20th-Early 21st C. development area type.

Topic	Sub-topic	Written analysis
Connections	-	Harvest Road serves as the main feeder route towards Wellington Place Estate in the northwest, whilst Alvington Way is the feeder route for Alvington Way Estate to the northeast - both routes lead off from Leicester Road. A series of winding local access roads branch off from the two feeder routes to provide access to smaller, quieter cul-de-sacs. Pavements are fitted along most roads, but tends to be narrow in width. A footpath runs from the canal towpath and extends throughout Wellington Place Estate. However, the two areas remain disconnected to the town centre and other amenities across town and so have high car dependencies. Walcot Road area to the south, on the other hand, has easy access to the town centre and a range of amenities, with direct footpath connections to the riverside, Welland Park and the town centre.
Built form	Urban form	There is strong overall consistency across development within this area type in Logan, where development share similarities in architectural style and use of material with subtle variations in layouts. Densities are typically high across all estates resulting in a tight-knit development pattern. Higher density apartments feature in Walcot Road area and Alvington Way Estate. The former has a more open and leafy feel where houses are fronting onto Welland Park. Most estates are exclusively residential, with the exception of Wellington Place Estate which is anticipating a community centre, primary school and local shops. Further housing will be delivered in Lubenham View Estate to the south of Wellington Place Estate.
	Block and plot sizes	Block and plot sizes for all three estates vary depending on housing typologies. Most units have on-plot parking but gardens tend to be smaller. On-street parking is more common in Walcot Road Estate, where plots are smaller and couldn't always accommodate for on-plot parking.
	Boundary treatments and setbacks	Setbacks and building lines are highly consistent within Wellington Place Estate where development are more homogeneous. Whereas setbacks are more varied across Alvington Way Estate and Walcot Road Estate where there's more variation in orientation and arrangement of houses on shorter cul-desacs. Boundary treatment range between hedgerows, shrubs and grass verges. Most properties have impervious surfaces and come across heavily hardscaped compared to other parts of Logan.
	Building size, scale, and type	Building scales and sizes vary across the three estates depending on housing typology - which ranges between 2-storey detached, semi-detached, short terraces and 3-4 storey apartments. Flat over garage units are observed in the Alvington Estate, whilst bungalows can be found in Walcot Road area.

Topic	Sub-topic	Written analysis
Nature	Green and blue infrastructure	Landscaping in the form of hedges, shrubs and trees form part of the green network for these areas. Pocket parks and amenity spaces across the estates also contribute towards the green infrastructure network, footpath connections from Wellington Place and Lubenham View Estates offer easy access to the countryside. Hedgerows and layered landscaping serve as important buffers along the edges of Wellington Place and Alvington Way Estates, allowing for a softer and more gradual transition into the surrounding countryside. River Welland runs to the north of Walcot Road Estate, which abuts Welland Park and the riverside promenade.
	Public realm/ open space	Streetscapes are generally cohesive across Wellington Place and Alvington Way Estates, given rise to the consistent building lines and homogeneous building styles. The presence of more mature planting and natural boundary treatment in both Alvington Way and Walcot Road Estates creates more visually interesting streetscapes, and softens visual impact of hardscaped areas and carparking. Market Harborough Country Park is a key recreational open space within easy access to Wellington Place and Lubenham View Estates, whilst serving as an effective protective buffer for the Grand Union Canal to the immediate east which is a designated Conservation Area. Pockets of amenity space and play area are also available in Alvington Way Estate, whilst Welland Park remains as the most accessible open space for Walcot Road Estate.

Table 09: Late 20th Century/Early 21st Century development area type description.



Figure 90: Properties within Alvington Way Estate, with rich boundary vegetation.

© Copyright Andrew Tatlow (Source: Geograph)



Figure 91: Bungalows in Walcot Road Estate. © *Oast House Archive (Source: Geograph)*



Figure 92: View towards Lubenham View and Wellington Place estates over Market Harborough Country Park.



Industrial/Employment

4.11 Industrial/Employment

Industrial and employment areas have a very different character and layout to residential areas. In Logan, they are located to the north close to the edge of the settlement, and are mostly accessed via the B6047. The largest of these clusters is Airfield Business Park. Others include a couple of garages fronting onto Leicester Road, and a Harborough District Council industrial site close the northern edge of Logan.



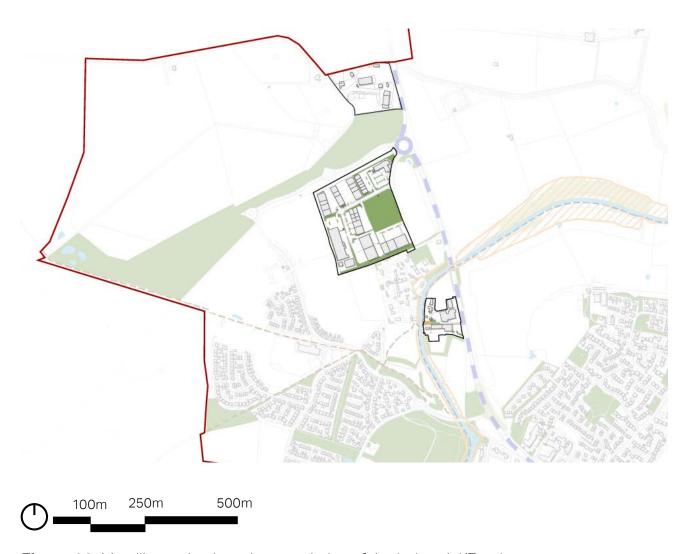


Figure 93: Map illustrating key characteristics of the Industrial/Employment area type.

Topic	Sub-topic	Written analysis
Connections	-	Most of these areas are accessed via the main road - Leicester Road (B6047). Smaller access roads branch off from the B6047 and are designed for the access of HGVs. There are parking provisions for both employees and visitors on all of these sites.
	Urban form	Tall buildings with large footprints are found in these areas due to the businesses requiring storage facilities, delivery bays and office/shopfloor space. Typical of industrial style employment areas, the urban grain is coarse which means that there are large buildings with generous spacing between them.
	Block and plot sizes	Due to the specific uses for businesses and industries in these areas, plots and blocks are larger than those found elsewhere in Logan.
Built form	Boundary treatments and setbacks	The parking provisions are bordered with low, dense hedgerows, with singular trees placed throughout to add visual interest and partially screen the large industrial buildings.
	Building size, scale, and type	Typical of industrial style employment areas, buildings are large in scale, with a uniform rectangular shape, designed to accommodate for warehousing uses. In addition to red brick, metal cladding of neutral tones, with a corrugated texture is used. These buildings have little to no decorative detailing as the focus is on practicality and functionality. The Harborough Innovation Centre is a 3-storeys office block which is designed for start up businesses.
Nature	Green and blue infrastructure	There is limited blue infrastructure in this area; however, the industrial site has trees and vegetation throughout, adding greenery to the heavily tarmaced space. The character area is also surrounded by agricultural land, with a woodland north of the character area boundary. The Grand Union Canal runs along the periphery of the Claas and Vauxhall garages.
	Public realm/ open space	There is little to no public realm or designated open space in these areas.

Table 10: Industrial/employment area type description



Figure 94: Harborough Innovation Centre © Copyright Dave Thompson (Source: Geograph)



Figure 95: Airfield Business Park to the north of Logan, with industrial warehouses currently under construction. *(Source: NPSG)*



The Woodlands

4.12 The Woodlands

The Woodlands area type is unique to Logan and its form is defined by meandering Grand Union Canal which runs along the periphery of the neighboruhood. Many of the houses back onto the canal, with direct access at the bottom of their gardens.



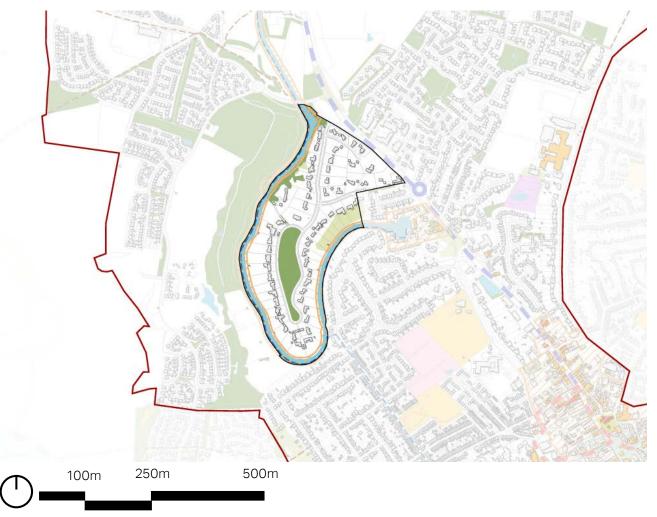


Figure 96: Map illustrating key characteristics of the The Woodlands area type.

Topic	Sub-topic	Written analysis
Connections	-	The Woodlands is a self-contained loop neighbourhood with short cul-de-sacs branching out of the main loop road. The area can only be accessed via the B6047/Leicester Road, providing onward connections towards the town centre and other areas of Market Harborough.
Built form	Urban form	This area is characterised by low density residential housing made up of large detached homes, along one road all orientated facing a large central green.
	Block and plot sizes	Large and spacious plots allowing for generous front and back gardens. Many of the back gardens extend the Grand Union Canal with most also having private moorings. The plot size is significantly larger than other parts of Logan.
	Boundary treatments and setbacks	Many properties have either low fences, hedgerows or other natural landscaping, maintaining the low density environment throughout the area. The properties have very large setbacks, with spacious front gardens and driveways.
	Building size, scale, and type	The detached properties expansive footprints with most being two or three stories tall. Properties are moderate in scale compared to their plot sizes, maintaining a low-density feel, ensuring the area does not feel overly built-up. Homes are typically constructed from brick and render, some with modern fenestrations and doors. Most properties also have large detached or semi-detached private garages.
Nature	Green and blue infrastructure	This area has a very green and leafy environment, due to the large open space which the houses all orientate towards. The area's leafy character can also be attributed to the predominantly green boundary treatments that are used throughout, maintaining a suburban feel to the area. Properties all back onto the Grand Union Canal so all have views and connections to blue infrastructure, this also contributes to the tranquillity of the area.
	Public realm/ open space	The Woodlands is characterised by tree-lined streets, pedestrian-friendly pathways, and well-maintained roadways, enhancing the public realm and encourages walking and cycling. The large central green space can be enjoyed by both residents and visitors as well.

Table 11: The Woodlands area type description.



Figure 97: Property with layered landscape screening to maintain privacy.



Figure 99: Property with step access to the canal © *Copyright Andrew Abbott (Source: Geograph)*



Figure 101: Hedgerow and fence screening at the boundary of a garden and the canal.



Figure 98: Properties in the Woodlands, set against a backdrop of mature trees along the Grand Union Canal. (Source: NPSG)



Figure 100: Private moorings at the bottom of the property's garden on the canal.



Figure 102: View of houses fronting onto the central green in The Woodlands. (Source: NPSG)



Non-traditional residential

4.13 Non-traditional residential

Located off of Leicester Road, this site hosts the Greenacres travellers site and has semi-permanent caravan homes with limited to no amenities, as it is completely occupied with park home plots of varying size and scale.





Figure 103: Map illustrating key characteristics of the Non-traditional residential area type.



5. Logan and Town Centre character area design guidance

The guidance in this section is specific to Logan and Town Centre and each of the character areas within it. This will ensure that any future development in the area is in keeping with the surrounding context, allowing Logan and Town Centre to retain its identity whilst meeting any development needs.

5.1 Introduction

The guidance will aim to ensure that any future development in Logan and Town Centre will be of a good quality while retaining the character of the part of the neighbourhood area that it is set in.

As displayed in the figure opposite, there are 10 area types under consideration in Logan and Town Centre. These are addressed individually in this chapter, with a brief description of their location, constraints and opportunities, followed by design guidance bespoke to the area type.

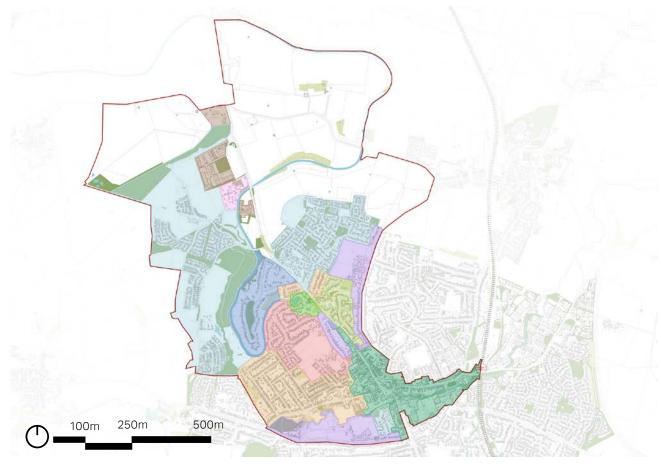
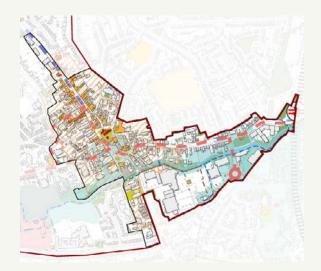


Figure 104: Logan and Town Centre area types (© Crown copyright and database rights 2024 Ordnance Survey AC0000808122)

5.2 Town Centre



Context

The Town Centre area is the historic core of Market Harborough Town, with most of the area being designated as a Conservation Area and a range of historically significant buildings serving as landmarks. This area is centrally located and is connected to other parts of the town via key routes such as Northampton Road, Leicester Road and St Mary's Road. A wide range of shops, services and amenities are located in this central area.

A vision for further development in the Town Centre area is outlined in the 2022 Market Harborough Town Centre Masterplan.

Design Guidance

The guidance below is applicable to the Town Centre area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 2.

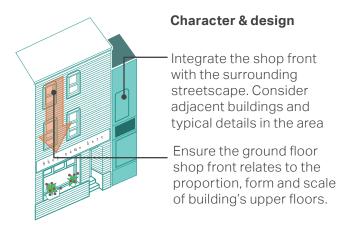
Public green spaces

 Public realm to proposals should support the implementation of the Market Harborough Town Centre Masterplan (2022).

Materials and architecture

- New development should provide a sympathetic response to the existing character and architectural details.
- New development should utilise the traditional materials that contribute to the local vernacular. Red brick and white or light coloured render are preferred.
- The design of shopfronts should be considered as a part of the overall composition of the building and the wider context of the town centre. The design of shop fronts should take into account the rhythm and character of the street such as the width of building, the horizontal or vertical emphasis, the variety of style and the architecture

of the building itself. Where the shop front continues to another building, a change in its design may be required;



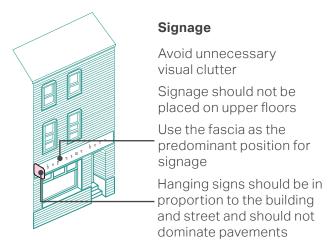


Figure 105: Shopfronts design principles

- On-street car parking in some areas leads to traffic issues which has a knock on effect on pedestrian safety and causing traffic congestion.
- While it is minimal, the River Welland does bring flood risk to the area in addition to the occasional surface water flooding.
- The presence of many listed buildings and the conservation area means that any infill development within close proximity would need to respect the setting of these structures.

Opportunities

- Encourage development to be in keeping with the historic material palette that has used in the Conservation Area.
- Retaining the existing building scale, massing and tight-knit pattern of development in the area.
- Activating backlands associated with long burgage plots, to create style development like Bennet's Place Courtyard.

- The fascia is the most important area
 of a shop front for advertising the
 business. Signage within the established
 proportions and confines of the
 fascia board should be maintained.
 Box signs or additional flat boards
 should be avoided as they create
 disproportionate depth and height; and
- Avoid using visually distinct sources of illumination that result in disproportionate signage, such as internally-illuminated box signs. Traditional hanging signs could also be acceptable.
 - Hanging baskets / plant boxes are encouraged to soften building frontages.
 - 2 Proportionate depth of signage not more than 1/4th of the total height of the shop front.
 - 3 Hanging signs are encouraged.
- Clear windows should account for 2/3rds of the shop front apart from window frames and mullions forming part of the vertical separation.
- Clear door access should account for at least 1/3rd of the shop front proportions. Provision of additional door access on wider shop fronts is acceptable.

- 2 CAFE CAFE
- Outdoor seating and displays are acceptable where pavement widths are 2m or more, can accommodate them and do not hinder pedestrian movement.
- 7 A-board signs are acceptable where pavement widths can accommodate them and do not hinder pedestrian movement.

Figure 106: Diagram illustrating example of high-quality shop frontage

Boundary treatment

 Natural boundary treatments should be prioritised over impermeable hardscaped boundary treatments where appropriate, mature trees within the Conservation Area must be retained.

Heights and rooflines

- Rooflines should be pitched with the use of traditional slates and pantiles.
- Buildings of thee storeys are common, some of the more interesting buildings are higher and the variety of heights are an important aspect of town centre character. Buildings of up to 4 storeys will be acceptable but if in the conservation area should be of traditional design to avoid some of the errors of the second half of the 20th Century.
- Any contemporary designs should blend modern materials and architectural forms that reflect the town's history and culture.

Plot layout

- Generally, façades must face the street and reinforce the existing building line.
- New developments should have subtle

- variations to improve visual interest but follow the building line;
- In addition to the preservation of historic assets supported by local plan policy, views and glimpses of St Dionysius Church from public spaces must be protected.
- Buildings and streets should be arranged to connect with the historic street pattern and movement network, ensuring direct access for walking, wheeling and cycling.
- The integrity of remaining burgage plots must be respected, although the sensitive redevelopment of yards behind town centre frontages is encouraged for town centre uses including residential.
- The fine grain of the Conservation Area must be maintained.
- Setbacks to the building line should be minimal – no more than 1m back from adjacent buildings – but commercial uses could allow for spill out space where practical.
- As a way to activate backlands in the Town Centre area, courtyards can be created to bring in daylight and to

reclaim additional green and public spaces as well as other amenities, existing examples such as Bennet's Place Courtyard and Corts Yards should serve as references for future backland development in the area.



Figure 107: Positive local example of a well-designed shopfront with large windows, timber fascia signage and outdoor seating.



Figure 108: Positive local example of backland activation providing housing at the rear of a burgage plot in a way that respects the existing built form.

AECOM

5.3 Historic Settlements



Context

Union Wharf is a Historic Settlement within Logan and is a vibrant mixed-use residential with restaurants and cafes. Its waterfront location along the Grand Union Canal provide a tranquil setting for apartment blocks that overlook the wharf, some of these had been converted from existing warehouses that are linked to the area's industrial legacy. Moorings provided within the wharf marina for narrow boats and barges.

Design Guidance

The guidance below is applicable to the Historic Settlements area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 03.

Public green spaces

- Any future development should be encouraged to provide frontage onto canal to offer natural surveillance to the area.
- Connectivity to the canal towpath should be enhanced where possible to maintain as a way to promote walking and active travel in the area, in order to minimise traffic and to maintain the tranquility of the area - this should include the provision of clear wayfinding signage.

Materials and architecture

New development should provide
 a sympathetic response to the
 scale of the existing development
 and be somewhat relatable to
 the existing architectural details,
 however contemporary takes on this
 architectural design can be acceptable.

New development should utilise the existing materials of red brick and black weatherboarding which contribute to the local vernacular.



Figure 110: Red brick with black timber frame casement windows and black timber doors make up the local vernacular to the area.



Figure 109: Apartment blocks mostly front onto the canal to offer passive surveillance to the area, thick hedges and shrubs typically used as boundary treatment.

- Inappropriate on-street parking in the area, especially during weekends where visitor flows peak can cause congestion to the area and result in a cluttered streetscape.
- Layout of the area meant that service and emergency vehicles access could be difficult.

Opportunities

- Preserving the tranquility and natural setting of the area fronting onto the Grand Union Canal.
- Enhancing and promoting car-free access to the area via the canal towpath as a way to minimise traffic to the area.
- Continue to encourage the adaptive reuse of existing warehouse buildings as a way to reinforce the unique character area and its past industrial legacy.
- Future development to remain inkeeping with the material palette, scale and massing of existing warehouse style buildings that are unique to the area.

Boundary treatment

- Vegetation typically in the form of green bushes and shrubs should be reflected in any form of development across the area in order to preserve the waterfront character of the area.
- Traditional iron railings, bollards and pillars associated to the industrial character of the area should be considered for future development.

Heights and rooflines

- Building heights should reflect the surrounding development (3-4 storeys) in order to remain in-keeping with the character of the area.
- Rooflines should be kept consistent and in-keeping with those of existing buildings.

Plot layout

 Generally, façades must either face the street or the canal to reinforce the existing building line and preserve natural surveillance. Gable ends of buildings can also be orientated to face the waterfront to reflect traditional canal side development arrangements.

Parking

- Due to the higher density in the area, parking courtyard is a commonly used parking typology for the area. These should be fitted with ample of landscaping to provide screening and to minimise visual impact of the parking areas from the streetscape.
- Cycle parking should be provided in the area around the wharf as a way to promote active travel access to the area from other parts of Market Harborough via the towpath.



Figure 111: Local examples of apartment blocks that face the waterfront with their gable ends.

5.4 Victorian/ Edwardian Development



Context

The Victorian and Edwardian terraces are concentrated in the New Harborough area, which serves as an extension to the Market Harborough town centre in Logan. There are further pockets of this development type to the north.

The formal, rectilinear layout of plots and narrow streets helps create a quiet, well-defined residential area. The buildings in the area are relatively homogeneous with strong architectural character that permeates the area.

Design Guidance

The guidance below is applicable to the Victorian/Edwardian Development areas type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 04.

Public green spaces

- Opportunities should be sought to incorporate street trees and vegetation in the public realm and along building frontages to increase the vegetation cover, larger mature trees can also serve as wayfinding landmarks.
- Connections towards Welland Park and pocket parks in the New Harborough area should be maintained to ensure easy access to these recreational spaces.

Materials and architecture

- New infill development should provide a sympathetic response to the existing character and architectural details;
- New development and renovations should utilise appropriate materials such as red brick and render to respect the original character of the buildings; and
- Window renovations should be sympathetic to the original designs of sash or bay windows.



Figure 112: Large street trees at street corners can sometimes serve as wayfinding cues and landmarks.



Figure 113: Red brick facade, paired with timber casement windows and decorative stone lintels are integral elements of the area's vernacular.

- Small block and plot sizes, and already densely built up constraining opportunities.
- In some areas, on-street car parking leads to traffic issues which has a knock-on effect on any pedestrians trying to cross the road at those points.
- Strong architectural uniformity to be maintained, constraining the style of new development.

Opportunities

- Preserve and enhance the historic character of the Victorian and Edwardian terraces.
- Retain the existing modest building scale and massing in the area.
- Only minor infill development should be considered for the area.

Boundary treatment

 Boundary treatments that are appropriate for the area such as low brick walls and front garden vegetation should be reinstated.

Heights and rooflines

- Rooflines should be pitched with the use of traditional grey slate.
- Building heights should reflect the surrounding development (2 storeys) in order to retain the existing character of the area.

Plot layout

- Generally, façades must face the street and reinforce the existing building line.
- New developments should have subtle variations to improve visual interest but follow the building line.

Parking

 Where possible, space should be provided for on-street parking. On-plot parking should not be incorporated as it goes against the existing character and design of plots.



Figure 114: Subtle variations in rooflines adds visual interest to the otherwise highly homogeneous streetscape resulting from the terrace typology.



Figure 115: Positive example of a corner unit which serves as a local shop and uses off-white render - effectively acting as a local landmark to area.

5.5 Linear Suburban Development



Context

Early 20th century suburban linear development occurred along Burnmill Road, Welland Park Road and Fairfield Road in Logan, resulted in a diversity of building styles, arrangements and typically larger houses. Development is characterised by properties arranged in a one-plot deep configuration along the roads. Most properties are equipped with generous front gardens and deep back gardens.

Design Guidance

The guidance below is applicable to the Linear Suburban Development area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 05.

Public and green spaces

- Grass verges and street trees should be maintained and reinforced in the public realm and along property boundaries to retain the leafy character of the area.
- Connections towards Welland Park should be enahnaced to maintain easy access from Welland Park Road.

Materials and architecture

- New development should provide a sympathetic response to the existing character and architectural details of surrounding properties.
- New development should utilise the typical materials of the area, including red brick and smooth render painted in light colours which contribute to the local vernacular.
- Triple casement windows and bay windows should be used wherever possible to match existing fenestration in the area.

Boundary treatment

 Natural boundary treatments such as hedges, shrubs, trees and grass verges should be the choices of boundary treatment for this area, these could be paired with low brick walls or low wooden fencing.







Figure 116: Typical boundary treatments include: wooden fencing paired with shrubs and grass verge (top), low brick wall paired with shrubs (middle) and variety of shrubs and landscaping paired with gravel paving (bottom).

- Linear pattern of development should be respected in order to remain in-keeping with existing character and form of the areas.
- Few east-west through routes, creating a barrier between this area and the rest of Logan, especially along Burnmill Road.
- Fast traffic speeds along these roads can affect pedestrian safety. This is a particular concern for Fairfield Road and Burnmill Road where there are schools and safety of students can be undermined.

Opportunities

- Large areas occupied by vegetation allow for opportunities to enhance biodiversity and sustainable drainage;
- Rolling topography along Burnmill Road meant that gaps between properties often open up to long views towards other parts of the settlement, making these views visible from the public realm; and
- Retain the diversity of building styles, scales, and massing in the area.

Heights and rooflines

- The design and materials of roofs should respect the character of existing buildings and rooflines; and
- Building heights should reflect the surrounding development (1-2 storeys) in order to retain the existing character of the area.

Plot layout

- Façades should face the street and reinforce the existing building line; and
- New developments could have subtle variations in setbacks but should follow the building line to retain the linear arrangement of the area.
- Any infill development along Burnmill Road should preserve gaps between buildings in order to maximise views out to surrounding areas as topography rises.

Parking

- Parking should be provided front-of-plot wherever possible without overwhelming front gardens; and
- Permeable materials should be used to decrease the risk of surface water runoff.



Figure 117: Gaps of buildings should be retained to maximise views along Burnmill Road.



Figure 118: Positive local example of onplot parking provided over permeable gravel paving.

5.6 Inter-war Suburbs



Context

Inter-war suburbs are located in the central part of Logan. Properties typically have regular sized front and back gardens. The overall consistency of plot layouts and built forms within the area is counterbalanced by occasional variations in building setbacks, the placement of small amenity spaces, and the looping layout of streets - creating visual interest along the streetscape.

Design Guidance

The guidance below is applicable to the Inter-war Suburb area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 06.

Public green spaces

- New development should provide landscaped front gardens and grass verges in order to increase the vegetation cover.
- Amenity spaces at the end of cul-desacs, such as those on Park Drive, should be well-maintained and should not be mounted by vehicles for parking.

Materials and architecture

- New development should provide a sympathetic response to the existing character and architectural details of the area.
- Casement windows and bay windows should be used wherever possible to match existing fenestration in the area.

Boundary treatment

 Vegetation in the form of low hedgerows, shrubs, together with low brick walls and wooden fences should be reflected in all development in the area.



Figure 119: Roofline of bungalows (yellow) are typically set below the treeline (green) which forms a natural backdrop to the properties.



Figure 120: Properties demonstrating positive use of typical boundary treatment in the area, to clearly delineate public from private spaces.

 Inadequate on-street parking can result in a cluttered streetscape, especially where vehicles are mounting pavements.

Opportunities

- Where possible, there should be subtle variation in building setbacks and rooflines throughout the area to retain visually interesting streetscapes.
- Integrate green space into the streetscape through grass verges and landscaped front gardens.
- Propose more adventurous building design that is still in keeping with the scale and massing of the existing development.

Heights and rooflines

- Building heights should be sympathetic to the surrounding development (1-2 storeys) and the sloping topography; and
- The height of buildings on the northern edge of the built-up area must remain below the tree canopy.

Plot layout

- Generally, façades must face the street to reinforce the existing building line and preserve natural surveillance;
- Any development at the edge of the area of separation must provide a green buffer to ensure a sympathetic transition with the fields to the north; and
- Plot layouts should consider the gentle changes in topography.

Parking

 On-plot car parking should be provided and located to the side of the property where possible to reduce inappropriate on-street parking which can clutter streets.

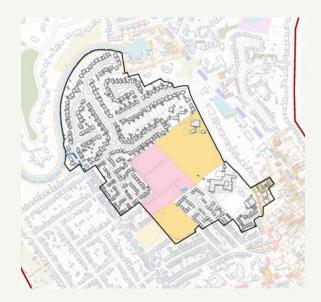


Figure 121: Primary facades of properties tend to front onto the street to provide passive surveillance.



Figure 122: Positive example of a property in the area with on-plot parking provided over permeable gravel paving, the use of natural boundary treatment effectively reduces visual impact of parking area on the streetscape.

5.7 Mid-20th Century Suburbs



Context

Significant areas of mid-20th century housing estates are located between the Grand Union Canal and the New Harborough area of Logan. These are mostly detached and semi-detached development with a mix of two-storey houses and bungalows. The area is defined by its swathes of winding streets and cul-de-sac, and a slight incline in topography northwards towards the canal.

Design Guidance

The guidance below is applicable to the Mid-20th Century Suburbs area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 07.

Public green spaces

- New development should provide landscaped front gardens and grass verges in order to increase the vegetation cover.
- Connectivity to amenity spaces such as Stevens Street Allotment and Logan Street Recreation Ground, should be enhanced where possible.
- Existing footpaths should be better maintained to link up with the Grand Union Canal and the wider countryside to create a network of green walking routes and promote biodiversity.

Materials and architecture

 New development should provide a sympathetic response to the scale of the existing development and be somewhat relatable to the existing architectural details. However, contemporary takes and tasteful adaptation of these architectural

- designs and styles can be acceptable.
- New development should utilise the existing materials, red brick, yellow brick and white weatherboarding, which contribute to the local vernacular.

Boundary treatment

 Vegetation in the form of low hedgerows, shrubs, flower beds, clipped grass lawns, together with low brick walls and gravel paving should be reflected in all development in the area.









Figure 123: Typical boundary treatments include: flowerbeds and shrubs over gravel (1), manicured shrubs and trees (2), hedgerows and low stone wall (3) and low brick wall with shrubs (4).

 Inadequate on-street parking can result in a cluttered streetscape, especially where vehicles are mounting pavements.

Opportunities

- Where possible, there should be subtle variation in building setbacks and rooflines throughout the area to retain visually interesting streetscapes.
- Integrate green space into the streetscape through grass verges and landscaped front gardens.
- Propose more adventurous building design that is still in keeping with the scale and massing of the existing development.

Heights and rooflines

- Building heights should be sympathetic to the surrounding development (1-2 storeys) and the sloping topography.
- Where dormers are added, these should be set below the roofline of the main building to remain subservient to the overall roofline.
- The height of buildings on the northern edge of the area must remain below the tree canopies of mature trees along the canal.

Plot layout

- Generally, façades must face the street to reinforce the existing building line and preserve natural surveillance;
- Any development at the edge of the area of separation must provide a green buffer to ensure a sympathetic transition with the fields to the north; and
- Plot layouts should consider the gentle changes in topography.

Parking

 On-plot car parking should be provided and located to the side of the property where possible to reduce inappropriate on-street parking which can clutter streets.



Figure 124: Positive local example of a house with a well-proportioned dormer that is set below the main roofline.



Figure 125: Properties with rooflines set below tree canopies are defining features for the character of the area to the north, Northleigh Grove.

5.8 Late 20th-Early 21st Century Development



Context

Large areas of housing built from the late 20th century onward are scattered throughout Logan, mostly in the form of infill or greenfield development. Site layouts and building typologies are varied to respond to the design characteristics of their immediate physical context. These sites contain neighbourhood parks, small areas of amenity green space and landscaping.

Design Guidance

The guidance below is applicable to the Late 20th-Early 21st Century Development area type in Logan and Town Centre.
The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 08.

Public green spaces

- Any future development should be encouraged to provide front gardens and grass verges to increase the vegetation cover.
- Green amenity spaces should be designed into any development to create a well joined up green network, these should be overlooked by surrounding properties for passive surveillance.
- Footpaths should be enhanced to create a network of green infrastructure in the area. Where possible, links to existing countryside footpaths should be created.
- Pocket parks should be designed into any proposed development to create a well joined up green network and enhance the community feel of Logan.

Materials and architecture

 New development should provide a sympathetic response to the scale of the existing development within Logan, and be somewhat relatable to the existing architectural details. However, contemporary takes and tasteful adaptation of these architectural designs and styles can be acceptable.

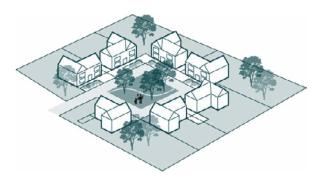


Figure 126: Diagram illustrating how open spaces and play areas should be designed to maximise passive surveillance.



Figure 127: Positive local example of a well-overlooked open space and footpath in Wellington Place Estate.

- Some cul-de-sac developments lack onward pedestrian connections.
- Some of the recently developed estates, such as Wellington Place and Lubenham View, can look overly homogeneous due to monotonous use of material and style of housing detracting from the overall character and distinctiveness of Logan.
- Overly hardscaped environment means that these areas are more prone to flooding from storm water runoff and a lack of permeable surfaces to attenuate the runoff.
- These large estates are distant from the town centre and are currently lacking in community amenities.

Opportunities

- Sustainable building design and Sustainable Urban Drainage mechanisms (e.g. rain gardens) should be prioritised where possible to help tackle climate change.
- Sufficient on-plot and visitor parking will help ensure that the streets of new housing developments are clear of clutter.

Connections and movement

- New development proposals should have connected layouts that are linked to the rest of Logan.
- Opportunities to provide pedestrian connections at cul-de-sac ends should be sought.

Heights and rooflines

- Building heights should reflect the surrounding development (1-3 storeys and take into account the topography of the site.
- Rooflines should be consistent but slightly varied to maintain a visually coherent and attractive streetscape.

Plot layout

 Primary building façades should generally face the street, with subtle variations to create interest in the streetscape.

Parking

- On-plot car parking should be provided and located to the side of the property where possible.
- Larger housing developments should be designed to consider visitor parking in a way that reduces on-street car parking.



Figure 128: Monotonous styles, use of material and rooflines meant that some of the recently developed housing estates could risk looking overly homogeneous, which detracts from their character.



Figure 129: Subtle variation in housing style, use of material and typology can add to the visual interest of streetscapes. The use of natural boundary treatment can also serve as screening to minimise visual impact of parking areas, Alvington Way Estate.

5.9 Industrial/ Employment



Context

The north of Logan contains several areas of employment zones. These are characterised by large plots which host buildings that are a range of sizes as a result of their different uses. The Leicester Road industrial cluster is smaller and more enclosed, consisting of garages. The Airfield Business Park consists of warehouses and Harborough Innovation Centre which hosts a range of offices. Lastly, a Harborough Distric Council industrial site is located on Gallow Field Road.

Design Guidance

The guidance below is applicable to the Industrial/employment area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 09.

Public green spaces

- Any future development should be encouraged to provide amenity space for workers;
- Where possible, enhancements to the walking and cycling environment should be sought; and
- Additional landscaping should be used to screen industrial buildings and reduce heat island effects.

Materials and architecture

 New development should provide a sympathetic response to the scale of the

- existing development and be somewhat relatable to the existing architectural details, however contemporary takes on this architectural design can be acceptable; and
- Opportunities to retrofit large industrial buildings with green roofs and/or photovoltaic panels should be sought.



Figure 130: View along the Wellington Place showing the Harborough Innovation Centre screened by trees.

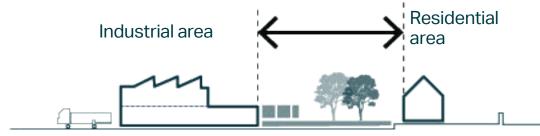


Figure 131: Use ancillary uses and landscaping to provide a buffer between residential and industrial uses

- Lack of open amenity space for employees to use.
- The streetscape and overall public realm are very car dominated.

Opportunities

- Key employment hubs that can provide local jobs.
- To encourage the development of open spaces to allow residents and workers to have outdoor amenity when they are on their break.
- To retrofit large industrial buildings with green roofs to manage stormwater, reduce heat island effects, and promote biodiversity.

Connections and movement

 Employment facilities should be designed to be accessible and easy to use for people using all reasonable modes of travel, including walking and cycling.

Parking

- Parking and service yards should be to the rear or side of employment buildings, allowing active frontages to directly address the public realm; and
- Opportunities to replace impervious road and parking surfaces with permeable paving and sustainable drainage solutions should be sought to reduce heat island effects and manage stormwater runoffs.



Figure 132: Positive use of layered landscaping at the edges of the site of Airfield Business Park to mitigate visual impact of carparking areas and large massing of warehouse buildings on surrounding residential areas.



Figure 133: Large commercial and industrial buildings present opportunities for green roofs or photovoltaic panels.

5.10 The Woodlands



Context

The Woodlands is a uniquely located development that is intrinsically linked to the Grand Union Canal, where most properties have long extensive gardens that extend up to the towpath. At the street level, there is a highly varied housing style, properties are large and are set on generous plots. With deep set back from the street, majority of the properties front onto a central green.

Design Guidance

The guidance below is applicable to the The Woodlands area type in Logan and Town Centre. The rationale for this is based on the constraints and opportunities of the area as well as the content of Table 10.

Public green spaces

- Grass verges and street trees should be maintained and reinforced in the public realm and along property boundaries to retain the leafy and open character of the area.
- Properties should be fronting on to the central green to maintain passive surveillance for the open space.

Materials and architecture

 New development should provide a sympathetic response to the scale and massing of existing development and be somewhat relatable to the existing architectural details, however contemporary takes on this architectural design can be acceptable.

Boundary treatment

 Natural boundary treatments such as hedges, shrubs, trees and grass verges should be used in this area to maintain its leafy character. These could be paired with low stone walls or low wooden fencing.







Figure 134: Typical boundary treatments include: long hedgerows (top), grass lawn paired with manicured shrubs and trees (middle) and shrubs paired with low stone wall (bottom).

- Due to the secluded location of the area, it remains disconnected from the rest of Logan and is therefore a highly car-dependent area.
- Boundary with the canal presents as a physical constraint to further development in the area.
- Insufficient landscape screening at boundaries between back gardens and the towpath could mean that privacy of properties are undermined.

Opportunities

- The leafy character of the area is in-keeping with its setting along the Grand Union Canal.
- Proximity to the Grand Union Canal towpath could be a precursor to encouraging more active travel.
- The area's unique setting along the canal adds to its distinctiveness as a neighbourhood in Logan.

Plot layout

- Primary building façades should generally face the street, deep setbacks and large gaps between buildings should be maintained throughout the area, in order to retain the area's open feel.
- Where the back of gardens abut the Grand Union Canal, these should be elevated from the canal-side to mitigate against flood risk. Landscape screening should be provided to buffer between back gardens and the canal to maintain privacy.

Connections, movement and parking

 On-plot parking should be provided to serve both residents and visitors. These areas should be shielded with landscaping to minimise visual impact to the streetscape.

Heights and rooflines

 New development proposals in these areas should reflect the existing typology and roofline and avoid overwhelming the open and low density character of the area.



Figure 135: The lack of layered landscaping meant that the property becomes highly visible from the towpath, which can undermine privacy. (Source: Geograph)



Figure 136: Positive use of layered landscaping to provide an effective landscape screen to reduce visibility of the property from the public realm.

5.11 Non-traditional Residential



Context

The Greenacre mobile home site is one of three sites within Market Harborough. It is accessed via Leicester Road and the site backs onto the Grand Union Canal.

Constraints

- Littering into the canal happens regularly, resulting in the degradation of its quality.
- Haphazard layout of the site and a lack of amenities make it undesirable for further development.

Design Guidance

The guidance below is applicable to the Non-traditional Residential area type in Logan and Town Centre. The rationale for this is based on the opportunities and constraints outlined for the area.

Public green spaces

 Where possible, links to existing countryside footpaths should be created.

Materials and architecture

New development should provide
 a sympathetic response to the
 scale of the existing development
 and be somewhat relatable to
 the existing architectural details,
 however contemporary takes on this
 architectural design can be acceptable.

Connections and movement

- Entrances and access points to the Greenacre site should be clear from obstruction, and should be clearly marked.
- Road width within the site should be appropriate for the access and movement of vehicles and pedestrians.

Heights and rooflines

New development proposals in these areas should reflect the existing typology across Logan, where possible there should be consistencies demonstrated in the form of buildings and rooflines.

Parking and utilities

- Sufficient parking and adequate parking arrangement should be provided within the Greenacre site, so that streetscapes do not come across cluttered.
- All properties should be equipped with adequate bin stores so that waste is disposed of and collected correctly.

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6. Checklist

This concluding section provides a number of questions based on established good practice against which the design proposal should be evaluated.

The checklist can be used to assess all proposals by objectively answering the questions below. Not all the questions will apply to every development. The relevant ones, however, should provide an assessment as to whether the design proposal has taken into account the context and provided an adequate design solution.

As a first step there are a number of ideas or principles that should be present in all proposals. These are listed under 'General design guidelines for new development'. Following these ideas and principles, a number of questions are listed for more specific topics.

1

General design guidelines for new development:

- Integrate with existing paths, streets, circulation networks and patterns of activity;
- Reinforce or enhance the established settlement character of streets, greens, and other spaces;
- Harmonise and enhance existing settlement in terms of physical form, architecture and land use;
- Relate well to local topography and landscape features, including prominent ridge lines and long distance views;
- Reflect, respect, and reinforce local architecture and historic distinctiveness;
- Retain and incorporate important existing features into the development;
- Respect surrounding buildings in terms of scale, height, form and massing;
- Adopt contextually appropriate materials and details;
- Provide adequate open space for the development in terms of both quantity and quality.

General design guidelines for new development:

- Incorporate necessary services and drainage infrastructure without causing unacceptable harm to retained features;
- Ensure all components e.g. buildings, landscapes, access routes, parking and open space are well related to each other:
- Make sufficient provision for sustainable waste management (including facilities for kerbside collection, waste separation, and minimisation where appropriate) without adverse impact on the street scene, the local landscape or the amenities of neighbours;
- Positively integrate energy efficient technologies;

- Positively integrate green infrastructure in accordance with national design guidance to positively contribute to liveability, biodiversity and climate change resilience;
- Ensure that places are designed with management, maintenance and the upkeep of utilities in mind; and
- Seek to implement passive environmental design principles by, firstly, considering how the site layout can optimise beneficial solar gain and reduce energy demands (e.g. insulation), before specification of energy efficient building services and finally incorporate renewable energy sources

2

Street grid and layout:

- Does it favour accessibility and connectivity? If not, why?
- Do the new points of access and street layout have regard for all users of the development; in particular pedestrians, cyclists and those with disabilities?
- What are the essential characteristics of the existing street pattern; are these reflected in the proposal?
- How will the new design or extension integrate with the existing street arrangement?
- Are the new points of access appropriate in terms of patterns of movement?
- Do the points of access conform to the statutory technical requirements?

Open environmental areas, views & character:

- What are the particular characteristics of this area which have been taken into account in the design; i.e. what are the landscape qualities of the area?
- Does the proposal maintain or enhance any identified views or views in general?
- How does the proposal affect the trees on or adjacent to the site?
- Can trees be used to provide natural shading from unwanted solar gain? i.e. deciduous trees can limit solar gains in summer, while maximising them in winter.
- Has the proposal been considered within its wider physical context?
- Has the impact on the landscape quality of the area been taken into account?
- In rural locations, has the impact of the development on the tranquillity of the area been fully considered?

- How does the proposal impact on existing views which are important to the area and how are these views incorporated in the design?
- Can any new views be created?
- Is there adequate amenity space for the development?
- Does the new development respect and enhance existing amenity space?
- Have opportunities for enhancing existing amenity spaces been explored?
- Will any communal amenity space be created? If so, how this will be used by the new owners and how will it be managed?
- Is there opportunity to increase the local area biodiversity?

4

Gateway and access features:

- What is the arrival point, how is it designed?
- Does the proposal maintain or enhance the existing gaps between settlements?
- Does the proposal affect or change the setting of a listed building or listed landscape?
- Is the landscaping to be hard or soft?

5

Buildings layout and grouping:

- What is the typical built pattern of buildings?
- How have the existing groupings been reflected in the proposal?
- Are proposed groups of buildings offering variety and texture to the townscape?

Buildings layout and grouping:

- What effect would the proposal have on the streetscape?
- Does the proposal maintain the character of dwelling clusters stemming from the main road?
- Does the proposal overlook any adjacent properties or gardens?
 How is this mitigated?
- Subject to topography and the clustering of existing buildings, are new buildings oriented to incorporate passive solar design principles, with, for example, one of the main glazed elevations within 30° due south, whilst also minimising overheating risk?

6

Building line and boundary treatment:

- What are the characteristics of the building line?
- How has the building line been respected in the proposals?
- Has the appropriateness of the boundary treatments been considered in the context of the site?

7

Building heights and roofline:

- What are the characteristics of the roofline?
- Have the proposals paid careful attention to height, form, massing and scale?
- If a higher than average building(s) is proposed, what would be the reason for making the development higher?
- Will the roof structure be capable of supporting a photovoltaic or solar thermal array either now, or in the future?
- Will the inclusion of roof mounted renewable technologies be an issue from a visual or planning perspective? If so, can they be screened from view, being careful not to cause over shading?

Household extensions:

- Does the proposed design respect the character of the area and the immediate neighbourhood, and does it have an adverse impact on neighbouring properties in relation to privacy, overbearing or overshadowing?
- Is the roof form of the extension appropriate to the original dwelling (considering angle of pitch)?
- Do the proposed materials match those of the existing dwelling?
- In case of side extensions, does it retain important gaps within the street scene and avoid a 'terracing effect'?
- Are any proposed dormer roof extensions set below the roofline of the original property?
- Does the proposed extension respond to the existing pattern of window and door openings?
- Is the side extension set back from the front of the house?
- Can any materials be reused in situ to reduce waste and embodied carbon?

9

Building materials & surface treatment:

- What is the distinctive material in the area?
- Do the proposed materials harmonise with the local materials?
- Does the proposal use high quality materials?
- Have the details of the windows, doors, eaves and roof details been addressed in the context of the overall design?
- Do the new proposed materials respect or enhance the existing area or adversely change its character?
- Are recycled materials, or those with high recycled content proposed?
- Has the embodied carbon of the materials been considered and are there options which can reduce the embodied carbon of the design?
 For example, wood structures and concrete alternatives.

10

Car parking:

- What parking solutions have been considered?
- Are the car spaces located and arranged in a way that is not dominant or detrimental to the sense of place?
- Has planting been considered to soften the presence of cars?
- Does the proposed car parking compromise the amenity of adjoining properties?
- Have the needs of wheelchair users been considered?
- Can electric vehicle charging points be provided?
- Can secure cycle storage be provided at an individual building level or through a central/ communal facility where appropriate?
- If covered car ports or cycle storage is included, can it incorporate roof mounted photovoltaic panels or a biodiverse roof in its design?

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