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# Fleckney

**Design Guidance and Codes**

**2025**

Delivering a better world

## Quality information

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Introduction

01

# 1. Introduction

**Through the Ministry of Housing, Communities and Local Government (MHCLG) Neighbourhood Planning Support Programme led by Locality, AECOM was commissioned to provide design support to Fleckney Parish Council.**

As the National Planning Policy Framework (NPPF) (paragraph 131) notes, 'good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities'.

Following an analysis of the Neighbourhood Area (NA), a set of architectural and design qualities will be identified. This set of qualities, combined with good design practice, will form the design guidance that development within Fleckney should follow in order to comply with this parish-wide design guidance and codes document.

## 1.1 Purpose of this document

This document sets out design guidance and codes based on the existing features of Fleckney. The document is intended to sit alongside the Neighbourhood Plan to provide guidance for applicants preparing proposals in the NA and as a guide for the Parish Council and Harborough District Council when considering planning applications.

### 1.1.1 What is Guidance versus Codes?

Design guidance identifies how development can be carried out in accordance with good design practice. Design codes are requirements that provide specific, detailed parameters for development. Proposals for development within the NA should demonstrate how the guidance has informed the design and how the design codes have been complied with. Where a proposal cannot comply with a code (or several) a justification should be provided.



**Figure 01:** The Golden Shield, Main Street.



**Figure 02:** The Old Crown, High Street.

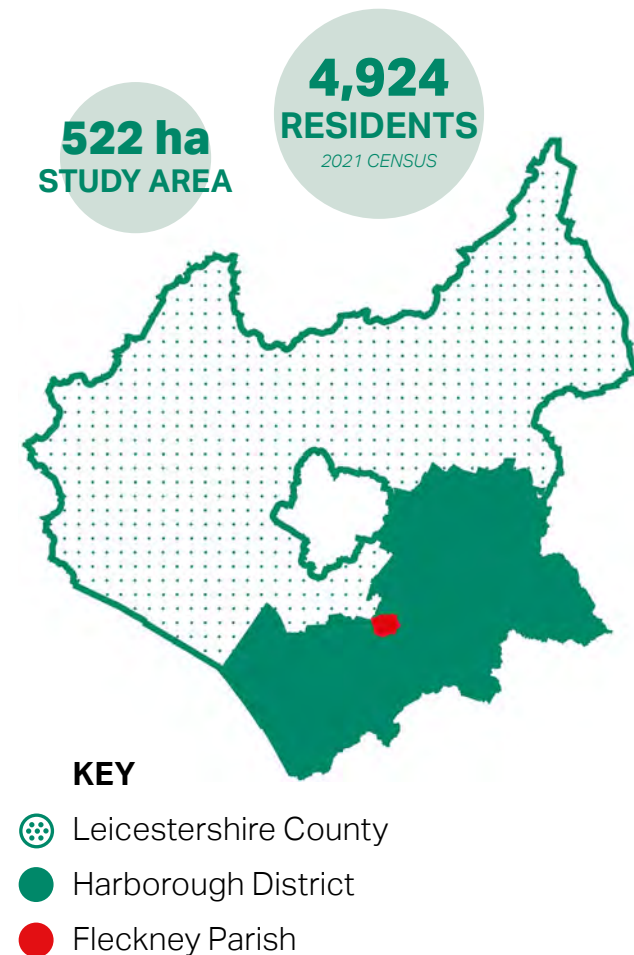
## 1.2 Area of study

Fleckney is a civil parish, overseen by the Harborough District Council as the Local Planning Authority (LPA). Fleckney is classed as a Rural Centre within the Harborough Local Plan 2011-2031 (Adopted April 2019).

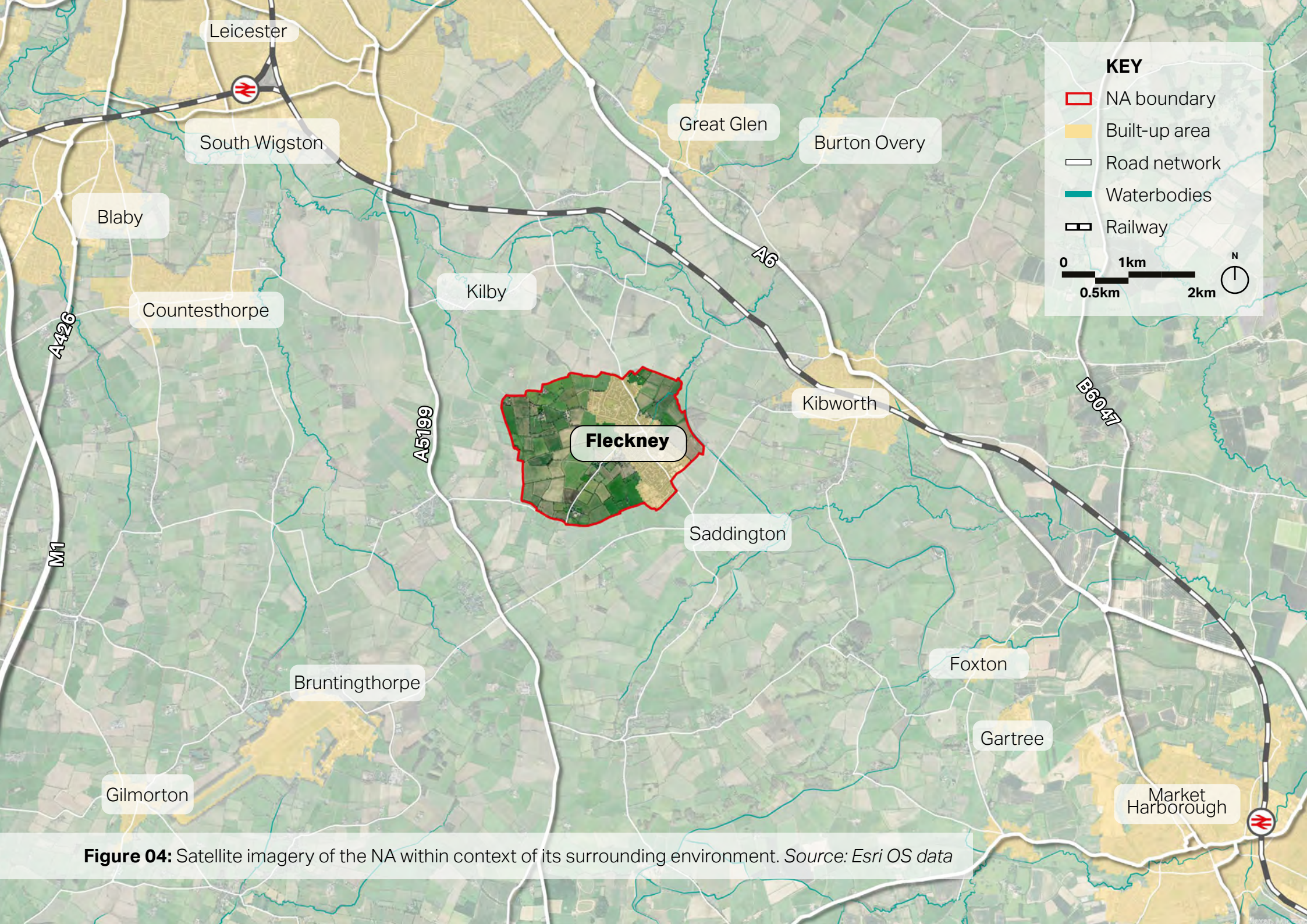
The Fleckney Neighbourhood Area (NA) is situated at the edge of the Harborough district, between Leicester and Market Harborough. Fleckney village has remained a predominately linear settlement through its development history, with Kilby Road, Leicester Road and Saddington Road remaining as the central routes. The village's setting is rural and it is connected to surrounding settlements via rural routes, with the closest main road being the A5199. The village has good public transport connections to London via Leicester and Market Harborough train stations, both approximately 10 miles from the village.

Fleckney has a rich history of agriculture until the 19th century, which saw the rise of the brickworks and later hosiery industry. The remains of these old factory buildings are still present at the edge of the village. The oldest settlement areas within the village are dispersed, including areas around the village centre surrounding St Nicholas Church and the Baptist Church along Main Street, and clusters of development along Gladstone Street and Victoria Street.

There are a number of local amenities within the village including the village hall, St Nicholas church, Baptist church, village shop, post office, pubs, restaurants, library, duck pond, and primary school.



**Figure 03:** The Neighbourhood Area in regional context. *Source: Esri OS data*



**Figure 04:** Satellite imagery of the NA within context of its surrounding environment. *Source: Esri OS data*

## 1.3 Process and engagement

A one-day site visit took place on 12th February 2025 commencing with an in-person meeting between AECOM and representatives of the Fleckney Parish Council to explore the group's key aims and objectives and to address any initial concerns.

This was followed by a tour of the parish, via car and on foot. This activity allowed consultants to appraise local character and the features informing its sense of place, such as heritage and landscape features. The exercise also provided valuable local insight into the area's pertinent design issues and opportunities, good and bad practice, as well the overall context for which the evidence-base of the Neighbourhood Plan will reflect.

This document has resulted from a collaborative effort between the Fleckney Parish Council and AECOM, reflecting the priorities of local residents. The design coding process includes the following steps:

AECOM



**Figure 05:** A brief chronological breakdown of the key elements and milestones used throughout the duration of the production of this document.

## 1.4 How to use this document

This document will be used differently by different people in the planning and development process.

A valuable way codes and guidance can be used is as part of a process of co-design and involvement that seeks to understand and takes account of local preferences for design quality. As such the codes and guidance can help to facilitate conversations to help align expectations, aid understanding, and identify key local issues.

The resulting design guidance and codes can then set out how to adequately respond to these issues in future development.

Design codes and guidance alone will not automatically secure quality design outcomes, but they will help to prevent poor outcomes by creating a rigorous process that establishes expectations for design quality.

What follows is a list of actors and how they will use the design Guidance:

Potential users	How they will use the design guidance and codes
<b>Applicants, developers, &amp; landowners</b>	As a guide to the community's and the Local Planning Authority's expectations on design, allowing a degree of certainty – they will be expected to follow the Guidance as planning consent is sought.
<b>Local planning authority</b>	As a reference point, embedded in policy, against which to assess planning applications. The guidance and codes should be discussed with applicants during any pre application discussions.
<b>Fleckney Parish Council</b>	As a guide when commenting on planning applications, ensuring that the guidance and codes are complied with.
<b>Local community organisations</b>	As a tool to promote community-backed development and to inform comments on planning applications.

**Table 01:** A list of potential users of this documents and how they will apply the design guidance and codes.

## 1.5 What makes Fleckney special?

Fleckney is a village with a distinct character, shaped by its rich heritage, natural landscape, and well-connected movement routes. Together, these elements create a strong sense of place, making Fleckney a unique and cherished community.

The village's history is reflected in its built environment, from its Listed Buildings to the 19th-century cottages that line its streets. Fleckney's industrial heritage, particularly its links to the boot and shoe industry, has played a significant role in shaping its identity, with former factory sites and workers' housing serving as reminders of its past. The designated Grand Union Canal Conservation Area, just beyond the parish boundary, once vital for trade and transport, continues to shape the area's heritage, providing an important recreational and ecological asset.

Beyond its built heritage, Fleckney is defined by its surrounding landscape, which enhances its rural identity. The village is home to a variety of greenspaces, from community parks to more natural

landscapes, offering valuable recreational opportunities. The presence of a Site of Special Scientific Interest (SSSI) highlights the area's ecological importance, supporting diverse wildlife. A well-established network of Public Rights of Way (PROW), including footpaths connecting to the wider Leicestershire countryside, allows residents and visitors to explore and appreciate the local environment.

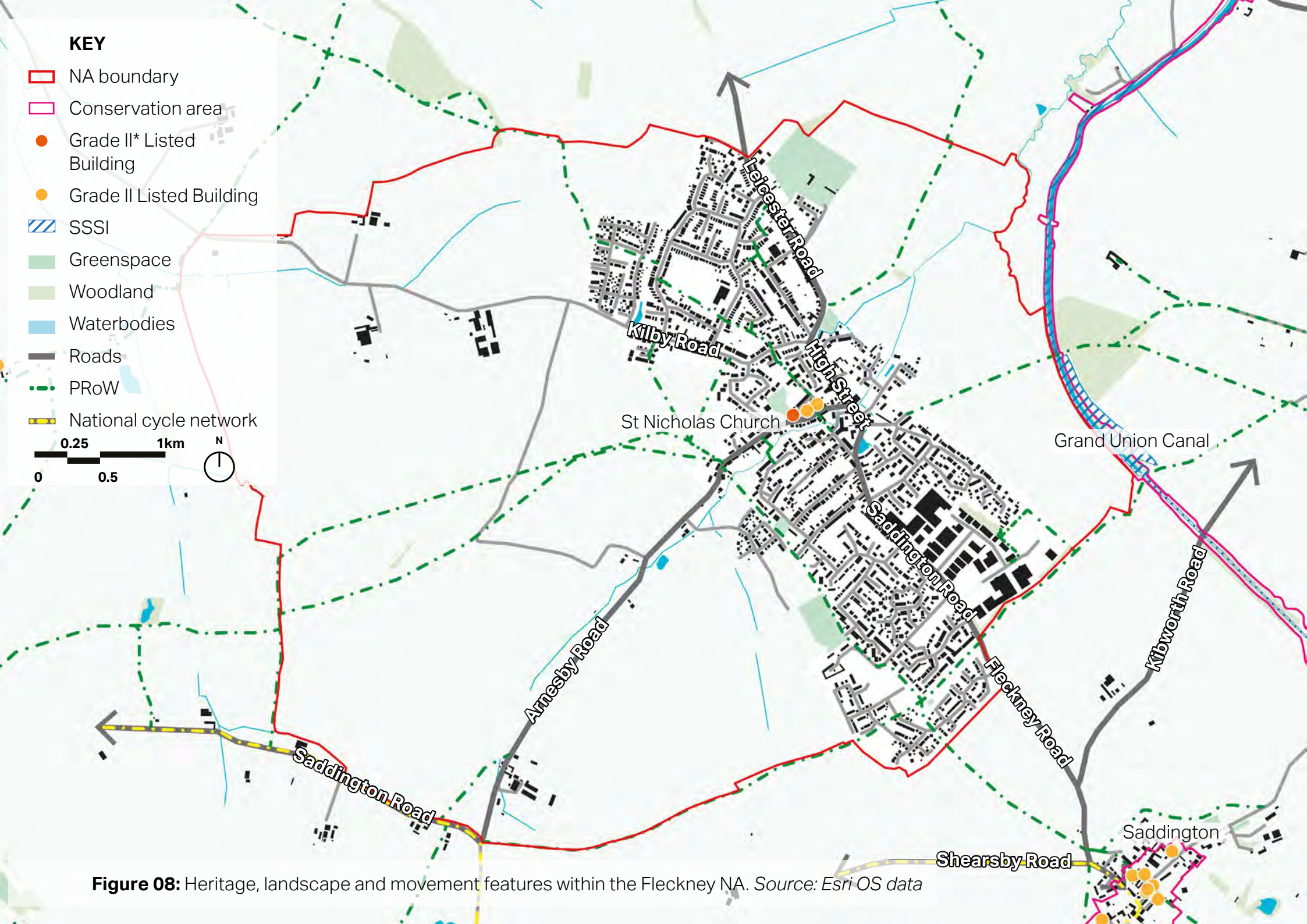
Movement within and around Fleckney is shaped by a well-defined network that balances accessibility and character. Key movement routes and gateways, such as those along Kilby Road and Saddington Road, define entry points into the village, reinforcing its sense of place. Public transport links provide vital connections to Leicester and Market Harborough, while the PROW network, including paths towards Saddington and the canal, encourages walking and cycling, fostering a stronger relationship between Fleckney's built and natural environment.



**Figure 06:** Fleckney Listed Buildings and cottages with a mix of materials add variety.



**Figure 07:** Consistent roof lines of historic cottages create a cohesive built form.



**Figure 08:** Heritage, landscape and movement features within the Fleckney NA. Source: Esri OS data

A photograph of a brick building with a green circular overlay. The building has a gabled roof with decorative wooden brackets. A sign above a window reads "Fleckney Silver Band". A small brown structure with a gabled roof in front of the building has "FSB" on it. The sky is overcast.

**Policy and Context  
Review**

**02**

## 2. Policy and Context Review

### 2.1 The vision and values and relationship to design quality

The Fleckney Neighbourhood Plan envisions a community that harmoniously blends its rich heritage with sustainable growth. The design code focuses on four key areas:

- **Preservation of Historic Character:** Protect and enhance Fleckney's architectural heritage, ensuring new developments respect existing styles and materials.
- **Community Connectivity:** Develop pedestrian-friendly pathways and communal spaces to foster social interaction and improve accessibility throughout the village.

- **Natural Environment Integration:** Ensure new constructions complement the natural surroundings, preserving green spaces and promoting biodiversity within the urban environment.
- **Sustainable Development:** Promote eco-friendly building practices, incorporating renewable energy sources and sustainable materials to reduce environmental impact.

These visions aim to create a cohesive, vibrant, and resilient community for current and future residents.

#### *Design Code Vision*

- *To protect the identity of Fleckney*
- *To protect important open areas*
- *To ensure good quality new homes provided for local housing needs*
- *To ensure that traffic flows well*

## 2.2 Planning policy context

### The NPPF 2024, paragraph 132 states that:

*'Plans should... set out a clear design vision and expectations, so that applicants have as much certainty as possible about what is likely to be acceptable. Design policies should be developed with local communities so they reflect local aspirations, and are grounded in an understanding and evaluation of each area's defining characteristics. Neighbourhood plans can play an important role in identifying the special qualities of each area and explaining how this should be reflected in development...'*

The Government is placing significant importance on the development of design guidance in order to set standards for design upfront and provide key principles regarding how sites should be developed.

Therefore this report's main objective is to develop design codes to sit alongside the Neighbourhood Plan to inform design

proposals within Masham Parish and ensure that they remain sympathetic to the surrounding character.

Other research, such as for the Government's Commission for Architecture and the Built Environment (now part of the Design Council; see, for example, *The Value of Good Design*<sup>1</sup>) has shown that good design of buildings and places can improve health and well-being, increase civic pride and cultural activity, reduce crime and anti-social behaviour and reduce pollution.

Therefore this document seeks to harness an understanding of how quality design can sensitively incorporate the best aspects of Masham's overall character into any future development.

Additionally, these following documents have informed the design guidance and codes within this report to ensure they are best aligned with the needs and opportunities identified for the NA:

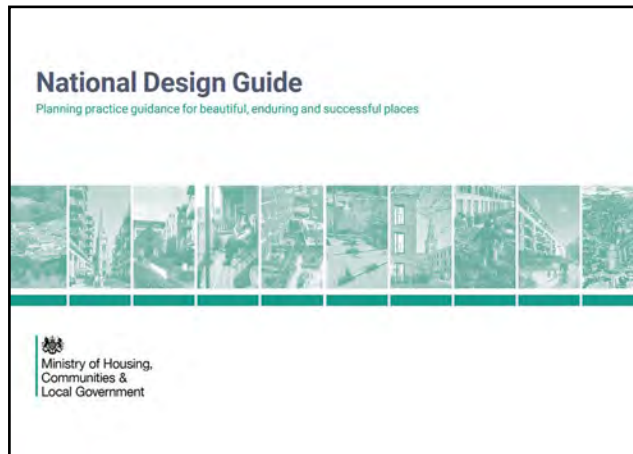
1. Available at: <https://www.designcouncil.org.uk/fileadmin/uploads/dc/Documents/the-value-of-good-design.pdf>

## National planning documents



### 2021 - National Model Design Code MHCLG

The National Model Design Code (NMDC) sets a baseline standard of quality and practice. It provides detailed Guidance on the production of design codes, guides, and policies to promote successful design. It expands on 10 characteristics of good design set out in the NDG.



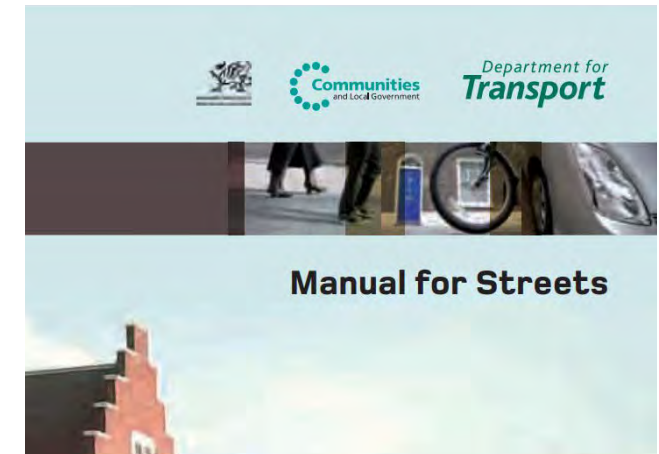
### 2019 - National Design Guide (updated January 2021) MHCLG

The National Design Guide (NDG) sets out 10 characteristics of a well-designed place and demonstrates what good design is in practice. As a companion document, it supports the ambitions of the NPPF to utilise the planning process in the creation of high-quality places.



### 2020 - Building for a Healthy Life Homes England

The BHL toolkit sets out principles to help local planning authorities to assess the quality of proposed (and completed) developments but can also provide useful prompts for planning applicants to consider during the different stages of the design process.



### 2007 - Manual for Streets Department for Transport

Development is expected to respond to the Manual for Streets, the Government's Guidance on how to design, construct, adopt and maintain residential streets. It promotes prioritising the needs of pedestrians and cyclists, whilst avoiding car dominated layouts.

## Regional and local policy guidance

### **Harborough Local Plan 2011-2031 - April 2019**

#### Harborough District Council

Harborough District Council (HDC) is the Local Planning Authority (LPA) for Welland. HDC adopted the Local Plan in April 2019. In the adopted Local Plan, Fleckney was considered a rural centre. Fleckney is considered as one of the largest amongst the rural centres with a significant village centre offering a range of shops and services. It is also considered to have substantial employment provision. Policy *H1 Provision of new housing* allocates Fleckney about 130 dwellings at land at Arnesby Road in accordance with Policy F1. *Policy BE1 Provision of new business development* allocates about 3 hectares in accordance with Policy F2 at land off Marlborough Drive. The Churchill Way Industrial Estate is identified as a key employment area and Victoria Works is identified as a general employment area within Fleckney. The Policies Map defines proposals for shopping and business uses, including mixed development with residential accommodation at Fleckney.

### **Harborough Emerging Local Plan**

#### Harborough District Council

The Emerging Local Plan Executive Summary published as of March 2025 outlines key development areas and site allocations, within which 150 new homes are allocated at Fleckney at Site Ref. F1 Land north of Kilby Road and land west Longgrey.

### **Planning Obligations SPD - 2022**

#### Harborough District Council

This SPD was adopted in June 2022. The document sets out the approach Harborough District Council takes to securing community infrastructure and affordable housing through the planning obligations process.

### **Development Management SPD - 2021**

#### Harborough District Council

This SPD was adopted in December 2021 and provides guidance to assist with the interpretation and implementation of the Local Plan. It covers important guidance on the design of new residential housing estates, extensions

to homes, conversions, shopfronts and advertisements. This design guidance should be consulted alongside this document.

### **Biodiversity Action Plan 2016-2026**

#### Leicestershire County Council

The Action Plan goes back to the broader intentions of the 1992 Biodiversity Convention - to halt the loss of biodiversity. The intention is to promote a new, more flexible approach to nature conservation and areas managed for wildlife in Leicestershire and Rutland which is relevant and applicable to all parts of the local landscape.

## 2.3 Reading the guidance and codes

The goal of these guidance and codes is to promote the best possible delivery of residential and public realm development, which will support sustainable and contextually appropriate designs.

If there is variation from the compliance requirements outlined in this document, it must be supported by factual evidence. Under such circumstances, developers and their design teams must show that the plan will produce a final proposal of the greatest quality that is consistent with the main goals of this document and, therefore, the goals of the Fleckney Neighbourhood Plan.

Submissions that do not adhere to this guidance, and that do not furnish strong rationales, supporting documentation and comprehensive examination of available solutions, may be refused.

The guidance and codes provided in the next section are arranged into area specific and area wide guidance and codes and


are supported by relevant analysis. These include detailed mapping, descriptions, diagrams and images taken from the NA and appropriate precedents.

Accompanying the guidances and codes are references to existing policies from Supplementary Planning Documents (SPDs) relevant to the local context. These support a nesting approach to link to relevant policies to ensure that there are no gaps in information and that all guidance and codes are bespoke to the context of Fleckney. Nested policies will appear throughout the next sections as shown below:

### Please note:

Both design codes and guidance are contained within this document, highlighted within boxes as shown here. The difference between codes and guidance is summarised below:

- Codes: Design codes are mandatory requirements for design issues and are expressed with the word **MUST**.
- Guidance: Design guidance set out aspirations for design that is expected to be delivered and are expressed with one of two words:
  - **SHOULD** reflects design principles that are strongly encouraged.
  - **COULD** reflects design principles that are suggestions.



Area Types Design  
Guidance and Codes

03

The Old Bank  
1  
Saddington Road

## 3. Area Types Design Guidance and Codes

This chapter presents a place analysis of the Fleckney Neighbourhood Area, setting out five area types. This helps to inform a series of design guidance and codes that are both sensitive and responsive to local context, landscape setting, and character.

### 3.1 Understanding place

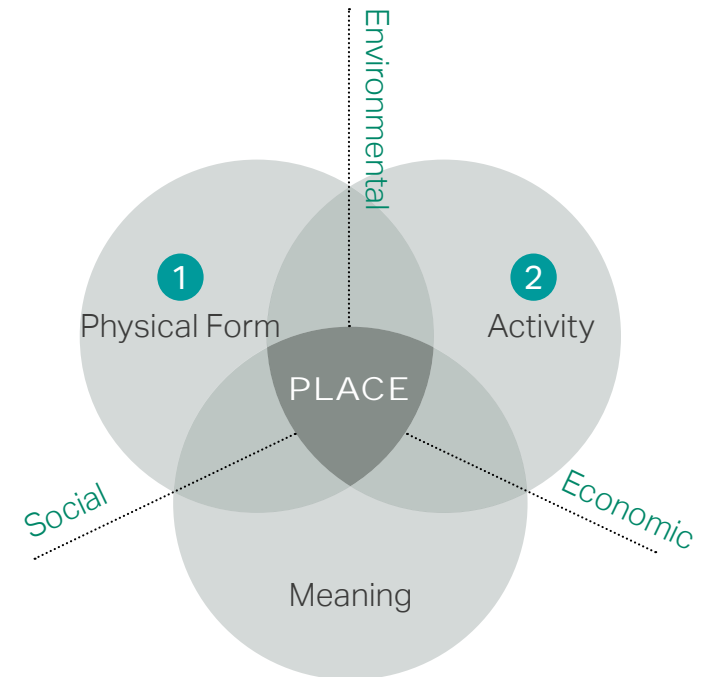
Achieving quality development starts with a comprehensive understanding of place. Places have a clear and strong identity and character. They are a combination of their physical form, their activities and their meaning to people. The adjacent diagram shows how these factors come together to create a successful place.

All new development must be informed by its own comprehensive analysis of place to understand a proposals broader context and establish aspirations and place-specific responses to the location, siting and design of new development.

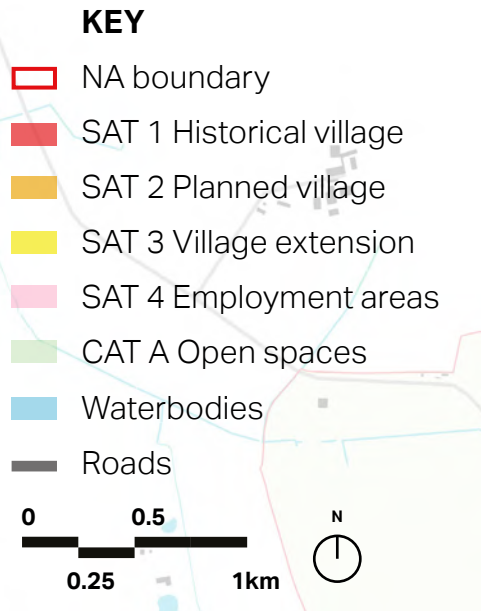
- ① Physical conditions of existing built development including layout, form, scale, appearance, landscape character, waterways and flood risk.
- ② Use, vitality and diversity, including community facilities and local services.
- ③ How a place is perceived, including local heritage, views inwards and outwards and social histories.

For the purposes of this document, a series of 'area types' have been identified for Fleckney, helping to illustrate the variation in character across the NA.

New developments should take note of the area type in which it is located, as each design proposal will require a tailored response based on its specific location.



**Figure 09:** A diagram showing how different factors come together to form a sense of place.



**Figure 10:** Boundaries of Fleckney's Area Types plan

## Settlement Area Types (SATs)

### SAT 1:

#### Historical Village

The Historical Village represents the early development of the village prior to the 20th century. All traditional features contribute to the central area of Fleckney and are an essential part of showcasing the village's history as well as its traditional appearance.

### SAT 2:

#### Planned Village

This area type is adjacent to the historic village, which has an organic and irregular layout with several roads radiating from the centre and urban edges. Most streets were developed in the middle and late 20th century.

### SAT 3:

#### Village Extension

The residential areas surrounding the traditional village were developed as extended communities after the late 20th century. These areas feature modern housing, and their design has been influenced by the 21st century to contemporary design principles. They form a distinct area type and thus contribute to local development.

### SAT 4:

#### Employment Areas

The areas to the south of Fleckney are mainly associated with employment land uses. These zones include agricultural activities and small scale production and they also form part of the economy.

## Countryside Area Types (CATs)

### CAT A: Open Spaces

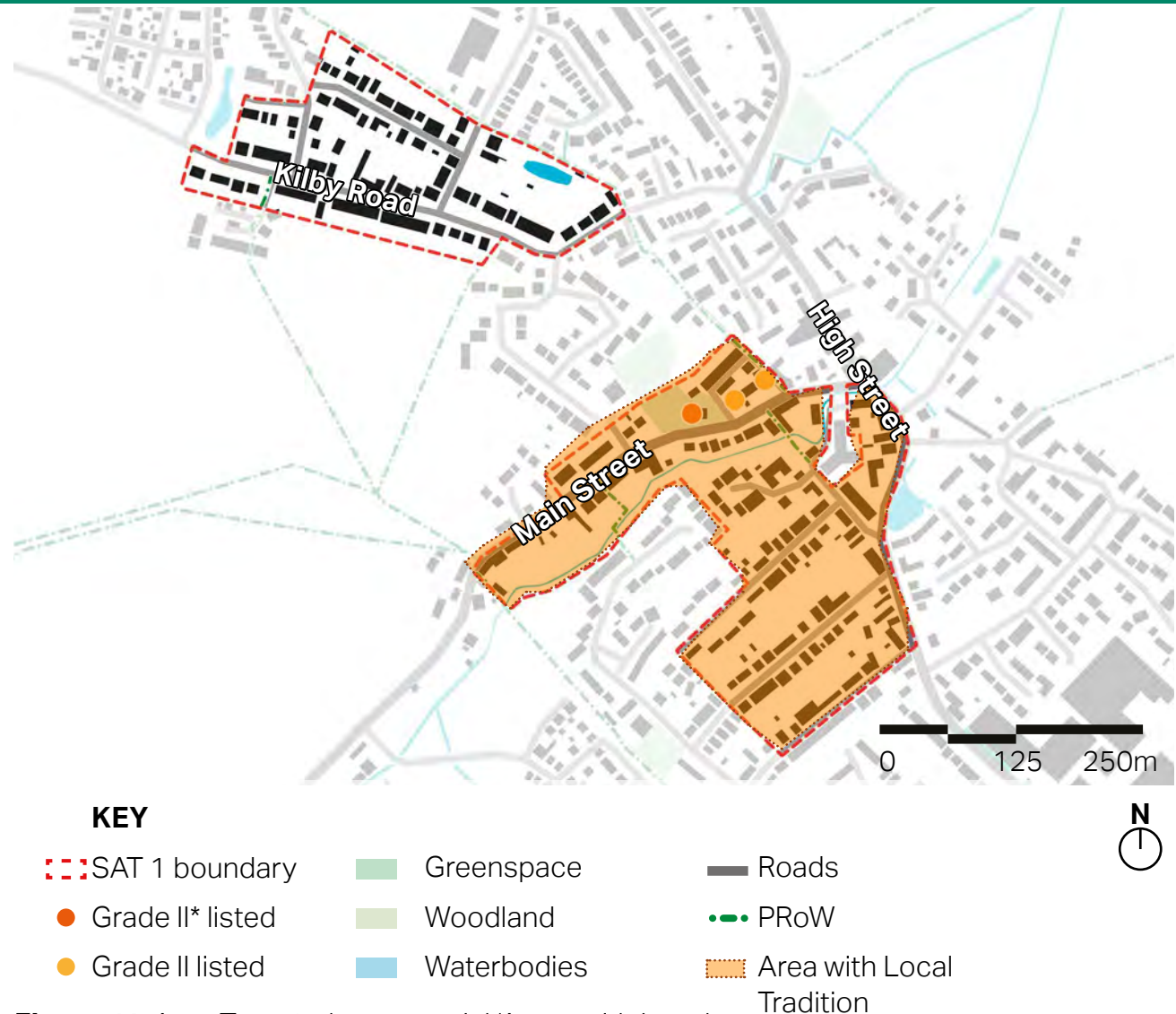
The open countryside around the village contributes to the rural ambiance of the parish, mitigating the visual effects on the natural surroundings.

## 3.2 SAT 1: Historical Village

### 3.2.1 Defining the area

The Historical Village (SAT 1) forms the heart of Fleckney, encompassing key areas such as Main Street, High Street, Kilby Road, Orchard Street, Victoria Street, and parts of Gladstone Street. These areas represent the village's earliest development, with an organic street pattern that reflects its historic growth. Defined by its traditional architecture, human-scale streets, and rich heritage. The Historical Village retains a strong sense of local identity and character.

As the oldest part of Fleckney, Main Street features a mix of Listed Buildings, 19th-century cottages, and historic brick-built structures, many of which were influenced by the village's industrial past. With buildings fronting directly onto the street, these urban form creates a distinctive sense of place, while surviving historic details—such as boundary walls, traditional shopfronts, and period materials—enhance the village's charm.



**Figure 11:** Area Type 1 plan, potential 'Areas with Local Tradition' boundaries

### SAT 1: Historical Village qualities and features

<b>Connection</b>	Movement networks	Organic and irregular street pattern, with key routes like Main Street and High Street serving as the primary movement corridors. Narrow roads and historic pathways create a walkable environment, connecting these core areas to surrounding residential and commercial zones.
<b>Built form</b>	Urban form	Compact and enclosed urban form, with buildings closely arranged along winding streets, reflecting its historic development. The street layout, varied plot sizes, and intimate scale contribute to a distinct sense of place and historic character.
	Building lines, boundaries and setbacks	Buildings are typically positioned directly at the street edge or with minimal setbacks, creating a strong and continuous frontage. Boundary treatments, such as low brick walls, iron railings, and hedgerows, define private spaces while maintaining visual connections to the public realm.
	Building, size, scale and type	Most buildings are two-story cottages or terraced houses, with some larger structures reflecting Fleckney's industrial past. Traditional architectural features, including brick facades, pitched roofs, and timber sash windows, reinforce the area's historic character.
<b>Nature</b>	Green and blue infrastructure	Although largely built-up, the area includes pockets of greenery in the form of private gardens, street trees, and small landscaped spaces. The proximity to surrounding countryside enhances access to natural assets.
	Open spaces and biodiversity	Small public open spaces, such as the village green and churchyard, provide valuable communal areas within the historic core. These spaces, along with mature trees and planting, contribute to local biodiversity and soften the built environment.
<b>Activity</b>	Uses and community	Mixed-use character, with residential properties interspersed with shops, pubs, and community facilities. This mix supports a vibrant village atmosphere, reinforcing Fleckney's identity as a historic and active rural settlement.

**Table 02:** Summary of the distinctive qualities and features that supplement the character of this Area Type.

### 3.2.2 Key Considerations and Good Features

#### Area with Local Tradition

The compact and irregular street network, with winding roads and buildings positioned close to the pavement, preserves Fleckney's historic charm while supporting a walkable village environment. Features such as narrow footpaths, historic passageways, and original brick boundary walls, particularly along Gladstone Street and Lamplighters, reinforce the intimate and enclosed nature of the historic core. Traditional iron railings, hedgerows, and low brick garden walls define property edges, maintaining a visual continuity that ties the streetscape together. Buildings along Main Street, High Street, Victoria Street, and Orchard Street feature traditional red brick with slate or tile roofs, reflecting the village's 19th-century origins and strong industrial heritage. Many of these buildings were historically linked to the boot and shoe industry, with some former workshop structures still visible, adding to the area's unique character.

Many buildings in SAT 1 retain craftsmanship details unique to Fleckney, including decorative brickwork, timber-framed sash windows, and original

chimneys, reflecting the skills of local builders from the late 19th and early 20th centuries. Notable historic landmarks, such as the St Nicholas village church, former coaching inns, and independent shops, remain active focal points, ensuring a continued connection to Fleckney's past.

The presence of long-standing local businesses, traditional public houses, and independent shops along Main Street and High Street sustains the area's historical mixed-use character. These establishments, along with community events and local gatherings, help maintain Fleckney's identity as a thriving, historically rich, and socially connected village center.

The Historical Village captures Fleckney's local tradition through its distinct architectural style, historic street layout, and enduring community landmarks. A potential 'Area with Local Tradition' is suggested to highlight the most valuable features of the village (see Figure 11).



**Figure 12:** Traditional red-brick terraced houses, featuring arched brick lintels, chimneys, and a strong street frontage.



**Figure 13:** Late 19th-century terraced houses with decorative brick detailing, original chimneys, and uniform building line

### Listed Building

Among the most notable listed structures is St. Nicholas' Church, which stands as a key historic landmark in Fleckney. Its historic masonry, traditional stained-glass windows, and ecclesiastical architectural detailing highlight its long-standing importance to the village's identity. Other Listed Buildings, including former farmhouses and cottages, retain original brickwork, slate roofs, and timber-framed windows, preserving the craftsmanship of the period in which they were built.

The protection of these buildings ensures that Fleckney's architectural heritage remains a visible and integral part of the village streetscape. Their presence enhances the historic character of SAT 1, reinforcing the importance of maintaining traditional materials and styles in new developments to complement the existing built environment.

### Design Code 1: Potential 'Area with Local Tradition' and Listed Buildings

- **New developments, extensions, and renovations in 'Areas with Local Tradition' should use traditional red brick, slate or clay tile roofs, and timber-framed windows to reflect the established character of the historic village;**
- **Buildings should maintain a continuous street frontage with minimal setbacks, following the existing pattern along streets like Main Street, High Street, and Orchard Street to preserve the historic enclosure;**
- **Boundaries should be defined using low brick walls, iron railings, or native hedgerows to complement the village's traditional street scene;**
- **Detailing such as arched brick lintels, decorative brickwork, and original chimney stacks should be retained and replicated where possible in new construction;**
- **Traditional shopfront styles should be preserved or reinstated, with signage and materials reflecting the historic commercial character of the village core;**
- **Any modifications to Listed Buildings or their settings must protect their architectural integrity, original materials, and historic features;**
- **Extensions to Listed Buildings should be subservient in scale, use high-quality materials, and respect the existing architectural form, avoiding modern additions that visually disrupt the historic fabric;**
- **Proposals adjacent to Listed Buildings must maintain appropriate scale, massing, and materials, ensuring they do not dominate or detract from the setting;**
- **Original roof pitches, slate or tile materials, and prominent chimney stacks should be retained and incorporated into new development to reinforce the historic silhouette of the village.**

## Unlisted Historical Features

There are numerous unlisted historical features that contribute significantly to the area's character and heritage. These unlisted elements, although not formally protected, are often integral to the village's narrative and can be found in various forms.

The narrow, winding streets, particularly around areas like Orchard Street and Gladstone Street, reflect the organic growth of the village over time. These unlisted streetscapes are essential in preserving the historical feel of the village centre. The way these streets radiate outward from a central core, rather than conforming to a grid pattern, is an essential feature of Fleckney's early settlement.

Many of the houses lining Main Street, High Street, and Victoria Street are not listed but still display features such as slate roofs, brick facades, and timber window frames that speak to the village's history. These buildings, often modest in their scale, represent the local vernacular architecture of their time and contribute to the area's historical charm.

Stone or brick boundary walls, often surrounding gardens or properties on streets like Lamplighters and Orchard Street, are common unlisted features. These walls have been an integral part of the village's landscape for centuries and help maintain the historic street demarcations. Many of these boundaries date back to the 18th and 19th centuries, aligning with the historical development of the village.

Unlisted features in the public realm, such as street furniture (e.g., benches, old-style street lamps), and traditional signage can add significant character to the area. For example, old-fashioned lampposts or cast-iron bollards, though not listed, can evoke a sense of time and place that is central to the village's identity.



**Figure 14:** Timber framed structure setback from the road with front gardens and driveways.



**Figure 15:** Detailing such as arched brick lintels, decorative brickwork, and original chimney stacks should be retained.



**Figure 16:** Detailed brick works on traditional buildings

## Design Code 2: Other historical assets

- Any new development or alteration of existing properties must follow the historical street grid and align with the natural curves and narrow width of the roads;
- Extensions or modifications should not disrupt the flow of streets or create excessive junctions or intersections that would detract from the historical layout;
- Pedestrian routes should be maintained and enhanced, ensuring they connect with key historical locations without introducing large-scale, urbanised pathways;
- New facades, windows, doors, and other external alterations must respect the scale, materials, and details of existing properties, including features such as sash windows, brick detailing, and traditional roof forms;
- Extensions should be modest in scale, avoiding overbearing or out-of-context designs. They must be compatible in materials and style with the traditional aesthetic of Fleckney's historic buildings;
- Original building materials (such as local brickwork, stone, or timber) should be retained and repaired where possible, with new materials chosen for their compatibility in texture and colour;
- Where boundary walls, gates, or fences are of historical significance (such as those made of stone, brick, or wrought iron), these should be preserved and maintained in their original form. Any repairs should be carried out using materials that match the original; and
- New boundary features should reflect the historic sense of enclosure, but not dominate the streetscape or obscure views of historically significant buildings or public spaces.

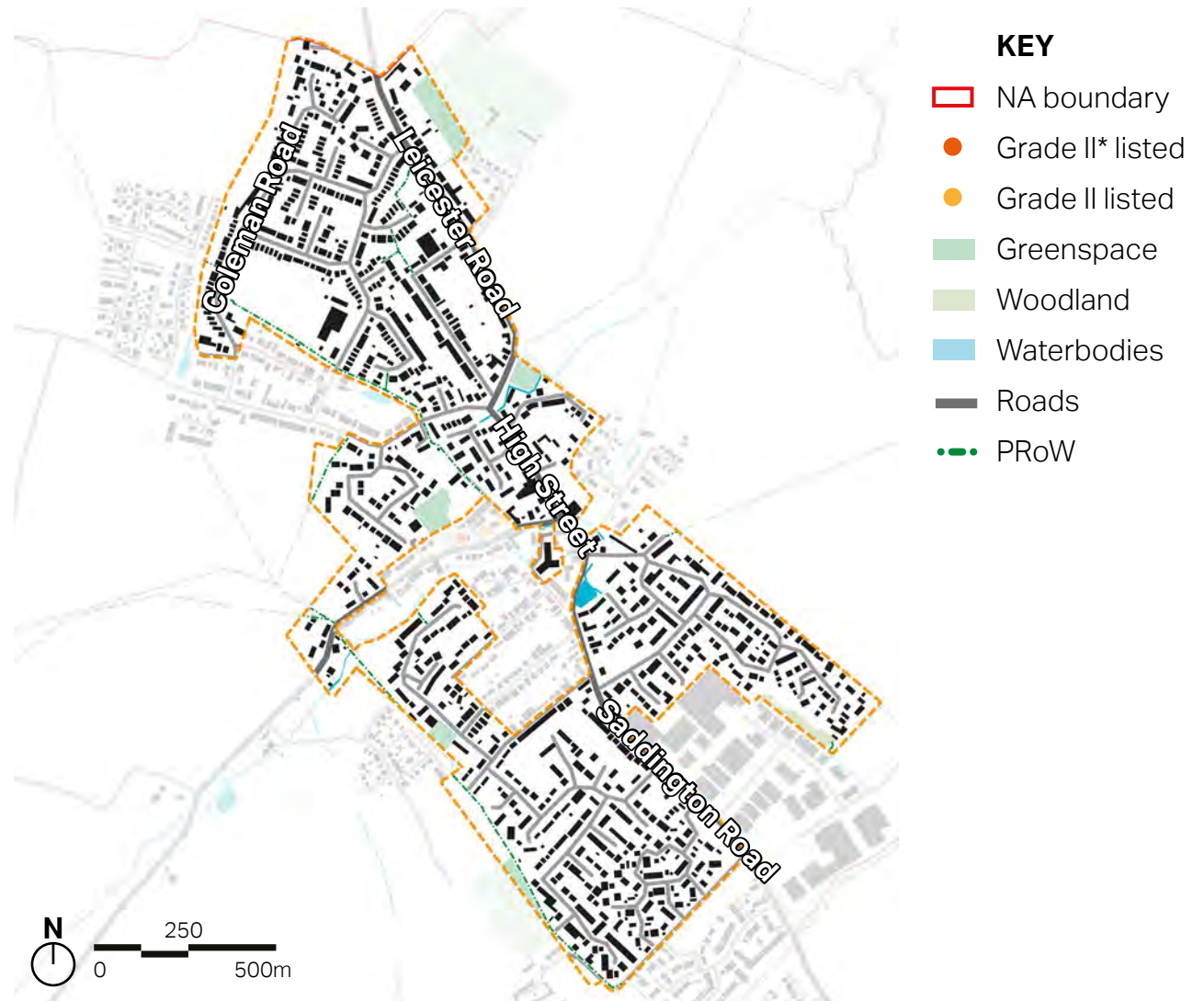
## 3.3 SAT 2: Planned Village

### 3.3.1 Defining the area

The Planned Village (SAT 2) is designed with a more structured and intentional layout, reflecting the increasing demand for housing and the post-war shift toward suburban development. These areas typically feature broader streets, more uniform housing styles, and a layout that accommodates modern transportation.

The housing in SAT 2 features typical mid-20th-century architecture styles, with semi-detached homes, terraced rows, and cul-de-sacs, often surrounded by generous front and rear gardens. Streets like Albert Street are lined with similar housing styles, while more recent developments like Lovelace Way and Cobwells Close bring a slightly more contemporary touch to the area.

Green spaces such as those along the Wranglands and Manor Road, and the proximity to the historical village center, create a subtle yet important connection between the new and old parts of Fleckney.



**Figure 17:** Area Type 2 plan,

## SAT 2: Planned Village qualities and features

<b>Connection</b>	Movement networks	Well-organised with a clear hierarchy of roads, including main routes like Leicester Road and Kilby Road, providing strong connections to the rest of Fleckney. These roads facilitate easy access to key destinations, and the use of cul-de-sacs and quieter residential streets creates a calm, pedestrian-friendly environment.
<b>Built form</b>	Urban form	Defined by a series of planned, residential streets that follow a relatively uniform, grid-like layout with some curvature along roads like Manor Road and Western Avenue. The area includes both post-war semi-detached housing and more recent developments.
	Building lines, boundaries and setbacks	Generally consistent building lines, with houses set back from the street to allow for front gardens and driveways, particularly along roads like Batchelor Road and Penclose Road. Boundary treatments vary, but traditional hedging and low brick walls are common.
	Building, size, scale and type	Predominantly two-storey, semi-detached and terraced homes, typical of mid-20th-century development, with a mix of brick and render finishes. Newer developments, such as those on Lovelace Way and The Meer, feature slightly larger, more modern detached homes, but still maintain a relatively low-rise, residential scale.
<b>Nature</b>	Green and blue infrastructure	Incorporates green and blue infrastructure through landscaped verges, tree planting along roads like Kilby Road, and green spaces such as those near The Wranglands. These features enhance the aesthetic value of the area and also provide essential ecological benefits, including rainwater absorption and habitat provision.
	Open spaces and biodiversity	Open spaces such as the communal green areas around Manor Road and Cobwells Close, contribute to the area's biodiversity by offering wildlife-friendly environments with ample green cover. These spaces provide areas for residents to enjoy nature while also supporting local flora and fauna.
<b>Activity</b>	Uses and community	Residential, with a strong sense of community fostered by the variety of housing types and the proximity to local amenities. The area is well-connected to the village center, encouraging community interaction, and features areas for recreational use, ensuring a mix of social, leisure, and residential functions.

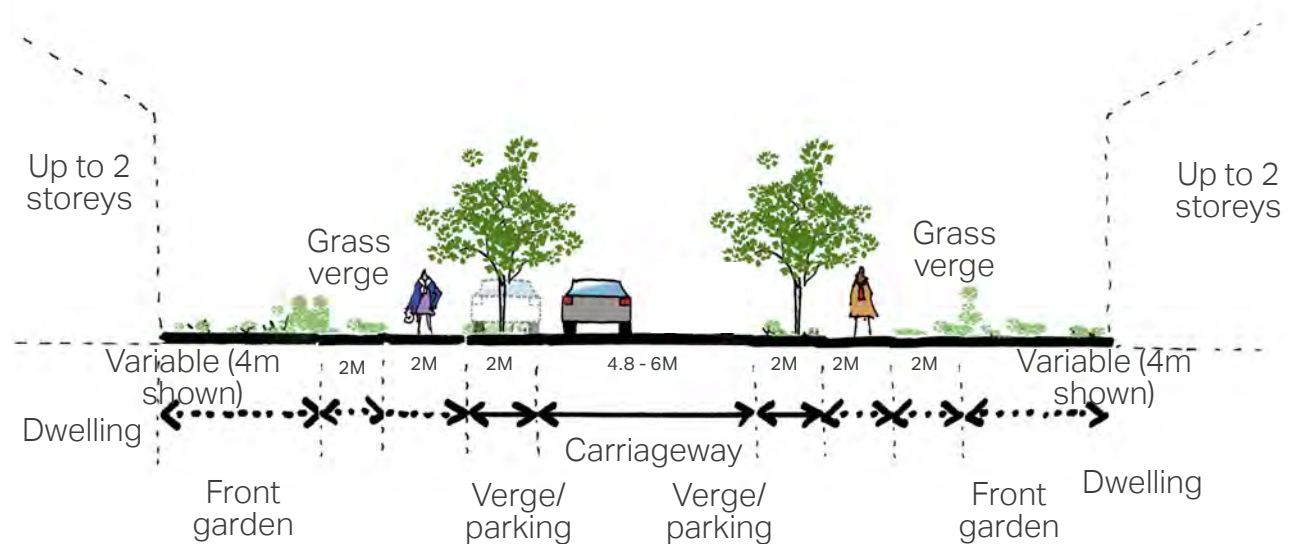
**Table 03:** Summary of the distinctive qualities and features that supplement the character of this Area Type.

### 3.3.2 Key Considerations and Good Features

#### Movement network

Fleckney maintains a small size but its road network provides adequate connectivity. Main Street and High Street function as the main routes that pass through the village center while Saddington Road and Kilby Road connect the area to the A6 and A5199. The roads create connections between the local area and surrounding towns and cities. The majority of roads in the village remain narrow and they cannot accommodate heavy traffic or large vehicles.

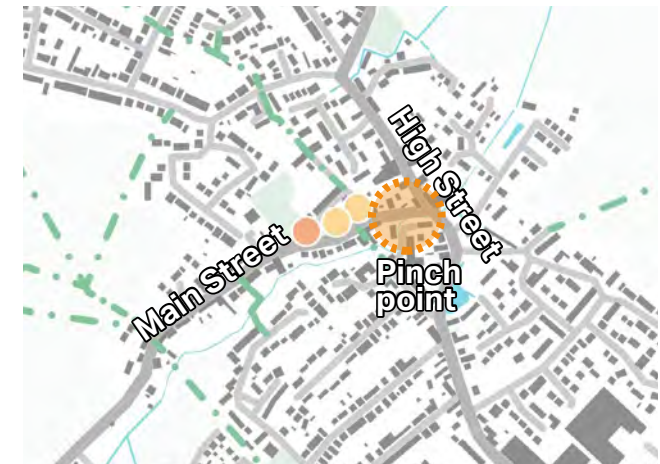
The problems caused by traffic have become more noticeable recent years. The intersection of Main Street and High Street functions as a well-documented traffic bottleneck that worsens during peak times because of on-street parking and limited carriageway space (Figure 20). Heavy goods vehicles increase network congestion and create safety risks for pedestrians and cyclists. The ideal street section illustration in Figure 18 aims to support future development needs.



**Figure 18:** An indicative of potential street section arrangement that can be considered.



**Figure 19:** Point of vehicular congestion at the junction of Main Street and High Street



**Figure 20:** Point of vehicular congestion



**Figure 21:** Existing streets with wide green verge and footpath



**Figure 22:** Existing streets with narrow green verge, narrowed by car parking

### Design Code 3: General Street

The general street is the prevalent street across from new development. The desired design features for this street type are:

- Where applicable and practical, speed limits should be 20mph with low traffic volumes and low speed and include design elements for traffic calming e.g. minimising the corner kerb radius, horizontal deflection and the like.
- Carriageways should accommodate two-way traffic and parking bays should be designed for cyclists to mix safely with motor vehicles.
- Front gardens should be well-planted to create an attractive environment.
- Preferably, locate parking to the side of the property to mitigate the impact of cars on the streetscape.
- If cars are parked at the front, at least 50% of the frontage should be landscaped and with a property boundary treatment.
- As part of Fleckney's defining character, street trees are important and also help to mitigate climate change. If this is not possible, front gardens should be deep enough to plant trees.
- Avoid using cul-de-sac solutions; instead use street furniture (e.g. bollards) to stop vehicle circulation whilst allowing other movement types.

## Current issues of movement network

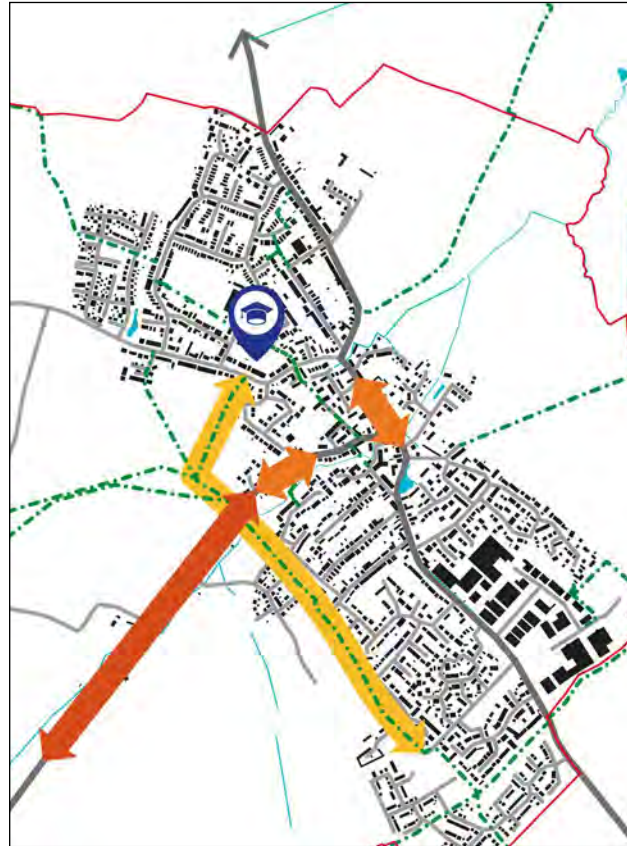
### Cycle routes

Cycle routes around the village, particularly along its edges, show signs of wear and are often too narrow, making them unsafe for cyclists. Some key routes, such as those leading to the school, require improvements to encourage safer and more sustainable travel. A lack of dedicated cycle infrastructure contributes to conflicts between vehicles and cyclists on busy roads.

### Footpaths

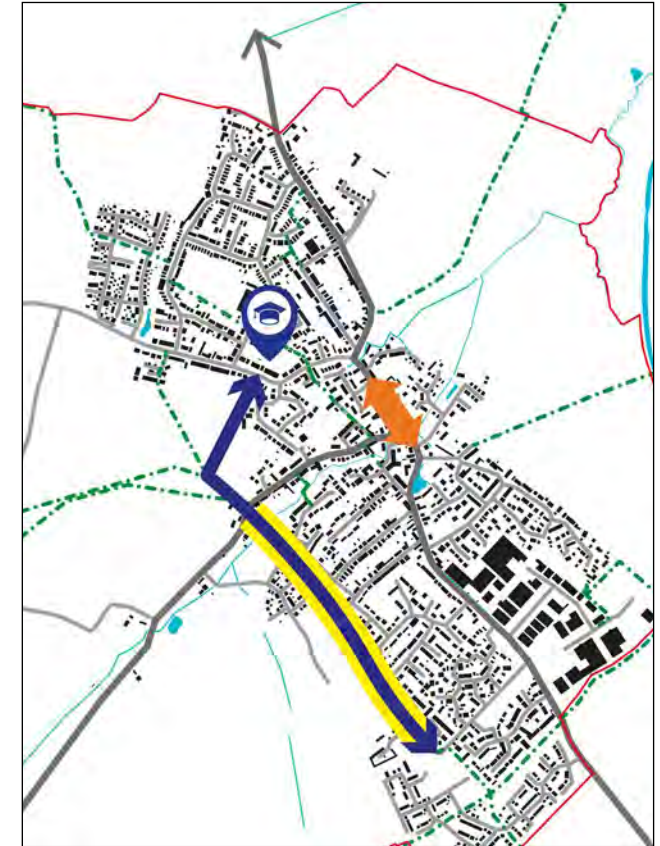
Footpaths leading to schools and other key destinations are in poor condition, with sections becoming muddy and unusable in winter. Some pavements, particularly in older parts of the village, are too narrow for safe pedestrian use, limiting accessibility for pushchairs and mobility aids. These factors create barriers to safe and convenient walking routes.

Active travel including cycle routes should be introduced and enhanced to deal with these issues.



**Figure 23:** Cycle routes

- ↔ Edges worn
- ↔ Too narrow
- ↔ Suggested cycleway to school



**Figure 24:** Footpaths

- ↔ Pavements too narrow
- ↔ Footpath to school  
Needs repair

## Design Code 4: Active Travel Movement Routes

Pedestrian and cycle routes should be encouraged and predominantly located to pass in front of buildings rather than behind them. All routes must be well overlooked, with opportunities for natural surveillance provided from adjacent buildings. All new residential developments should have regards to the location, spatial requirements and aesthetic of these features.

- Pedestrian and cycle routes should be designed to be accessible by those with both full and restricted mobility. Careful attention should be afforded to the use of street clutter that can block or impede routes for those in wheelchairs, or those pushing prams or pushchairs.
- Safety and hydrology should be considered to ensure the functions of the canals can be delivered in a long term.



**Figure 25:** Examples of segregated cycle routes and allocated cycle routes along roads

## Design Code 5: Cycle Routes

- **Connectivity:** Cycle routes should integrate seamlessly with pedestrian and vehicular networks, ensuring access to key destinations within 5 km. Shared paths must be at least 3 metres wide, with clear markings to avoid conflicts.
- **Safety:** Routes should ensure visibility of 25 metres at junctions, feature lighting for night use, and maintain gradients below 1:20 for accessibility. Priority crossings must enhance cyclist safety.
- **Sustainability:** Use permeable surfaces for drainage, incorporate green buffers for separation, and align designs with Fleckney landscape. Native vegetation and aesthetic integration should reflect local character while supporting ecological balance.

### Lack of local Identity

The heart of Fleckney maintains its character; however the surrounding areas seem to blend into the landscape losing touch with the villages rich history and individual identity. The neighbouring regions are predominantly filled with neighbourhoods and residential projects that do not showcase any distinct local charm or traditional elements. These 20th century structures prioritise functionality and consistency featuring brick exteriors and standardised designs reminiscent of areas, in the UK. These areas lack characteristics specific to Fleckney.



**Figure 26:** Simple red brick buildings with pitched roofs can be found in many England towns, lack of local identity



**Figure 27:** Example of existing arrangement of typical residential streets in the Area Type

A typical residential street in the UK often features front gardens measuring 5–7 metres, accompanied by 2-metre-wide footpaths on either side of the road. Such layouts are prevalent across England but often lack a distinctive local character.

## Design Code 6: Enhancing Local Identity

- **Emphasis traditional architectural styles incorporate local architectural elements in new builds, like brick facades, pitched slate roofs, windows, and dormers, to seamlessly blend with Fleckney's traditional village aesthetic.**
- **Preserve original details in historical villages and restore features like timber-framed doors, metal railings, and proportions of sash windows using traditional materials, maintaining the historic charm and visual consistency of Fleckney's historic areas.**
- **Reflect local greenery in landscaping use native plants, gravel or brick pathways, and low stone walls. Natural hedges and rural-style fencing enhance cohesion with Fleckney's green village environment.**

## 3.4 SAT 3: Village Extension

### 3.4.1 Defining the area

The Village Extension (SAT 3) area of Fleckney represents the more recent phases of development, with newer residential areas located along streets such as Garner Way, Furnival Close, Bromley Close, Freer Road, Howlett Road, Badcock Way, Gamble Avenue, Tigers Road, and the recently constructed Davidson's Homes development.

These areas were predominantly developed in the late 20th and early 21st centuries, and they serve as a natural expansion of the village, accommodating the growing population and offering modern housing options. Unlike the more historical and planned areas of Fleckney, SAT 3 is characterised by contemporary housing designs, wider roads, and a more formalised approach to design. The development in this area continues the trend of creating a balanced and sustainable village, providing a mix of family homes and green spaces, while maintaining close proximity to both the historical village and surrounding countryside.

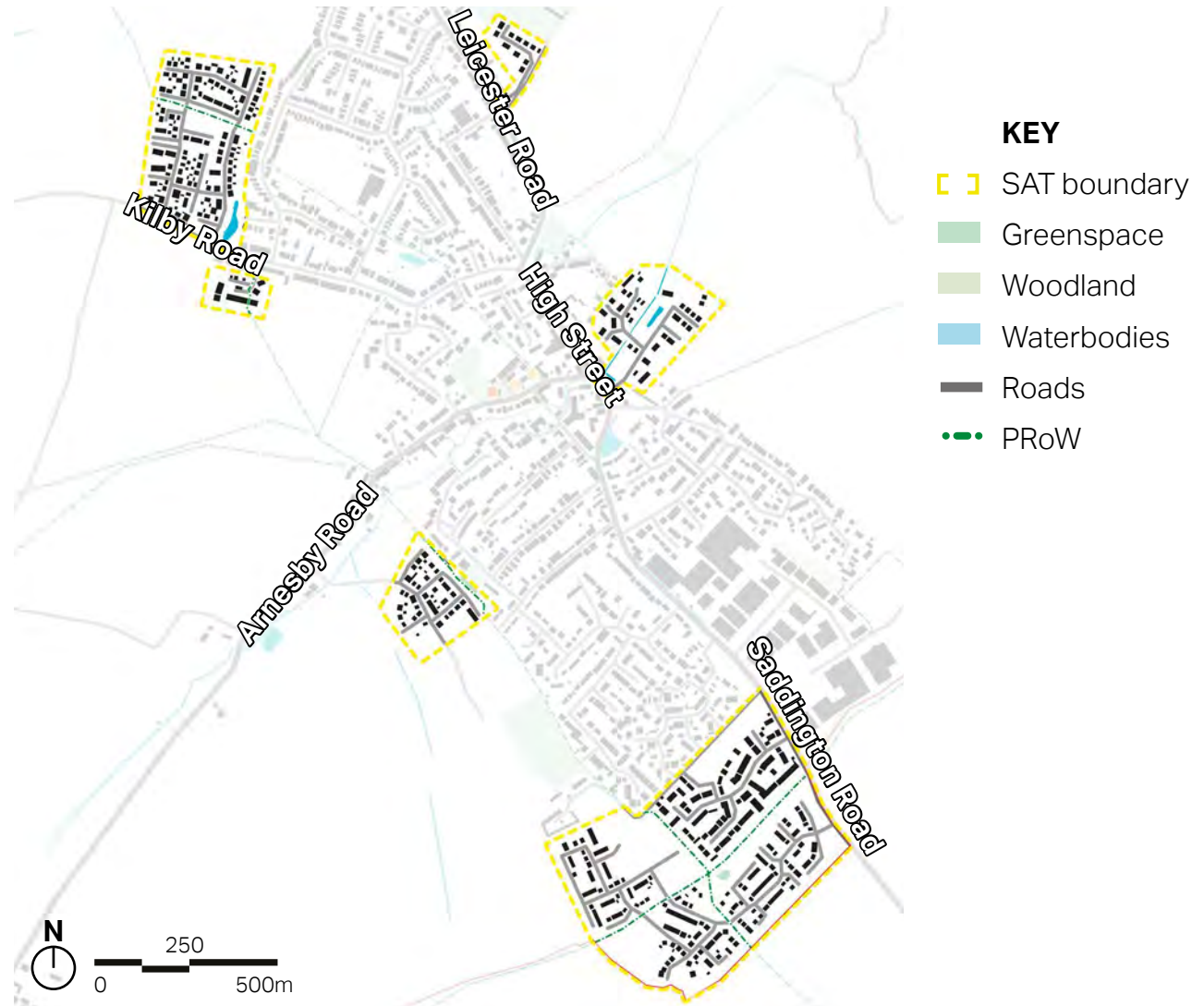


Figure 28: Area Type 3 plan

### SAT 3: Village Extension qualities and features

<b>Connection</b>	Movement networks	Defined by well-connected streets such as Freer Road and Howlett Road, which provide easy access to Fleckney's village centre and the surrounding countryside. Streets like Tigers Road, Gamble Avenue, and Badcock Way are designed with cul-de-sacs.
<b>Built form</b>	Urban form	Modern, planned layout, with residential streets like Garner Way, Furnival Close, and Bromley Close offering a mix of detached and semi-detached homes.
	Building lines, boundaries and setbacks	Buildings are typically setback from the streets, creating front gardens and driveways, particularly evident along roads like Howlett Road and Gamble Avenue. Boundary treatments typically include low brick walls, hedging, and timber fencing, contributing to an open and cohesive streetscape while providing clear demarcations of private and public space.
	Building, size, scale and type	Predominantly features large, two-storey homes, with detached houses and semi-detached properties lining streets like Freer Road and Bromley Close. Developments such as Davidson's Homes feature spacious designs and large gardens.
<b>Nature</b>	Green and blue infrastructure	Tree-lined streets such as Howlett Road and Tigers Road, and green spaces near Garner Way and Badcock Way, contribute to the area's aesthetic and environmental sustainability. Sustainable water management systems, like surface water drainage and natural landscaping, enhance the ecological value of these developments.
	Open spaces and biodiversity	Open spaces such as those near Freer Road and The Wranglands, provide ample recreational opportunities for residents, with green areas supporting local wildlife. The development of these spaces incorporates native planting and tree cover, fostering biodiversity.
<b>Activity</b>	Uses and community	Primarily residential, with a strong sense of community facilitated by communal green spaces. The design of these areas, with their pedestrian-friendly streets and local parks, encourages interaction among residents.

**Table 04:** Summary of the distinctive qualities and features that supplement the character of this Area Type.

### 3.4.2 Key Considerations and Good Features

#### Frontage to Countryside

While development in recent years of the Village Extension Area Type has encroached into surrounding countryside, new developments have formed a positive relationship with open fields and natural landscape. This is exemplified by the abundant and often large green spaces and green infrastructure interventions contained within these developments.

Particularly within developments to the south, large open green spaces or more natural green infrastructure features help to effectively transition buildings with the surrounding countryside. Furthermore, dwellings positively address surrounding green spaces or countryside.

This provides natural surveillance while also creating positive townscape views when approaching Fleckney in Elmet from a distance, reinforcing the settlement edge to surrounding countryside. Such features should be retained and enhanced by any proposed developments.

The buildings are arranged in a relaxed, informal manner, allowing for views of open spaces that help maintain the rural character. Front gardens and roads, including shared surfaces, are well-defined with clear boundary treatments.



Well-kept open spaces with footpaths create a welcoming atmosphere and serve as a natural buffer between the countryside and the village.

**Figure 29:** A good example of arrangement of natural environment frontages

Open spaces with play facilities encourage people to spend time outdoors.



The area is defined by well-organised buildings that face open spaces, creating a central feature of its character.

**Figure 31:** Good example of arrangement of open spaces frontages

## Design Code 7: Natural environment frontage

- New development should maintain low-density layouts, using traditional materials and designs that reflect Fleckney's rural heritage. Preserve existing hedgerows and mature trees to blend development with the countryside.
- **Integration with Nature:** New developments should include green buffers, such as tree-lined streets and open spaces, connecting seamlessly to the surrounding countryside. Incorporate native planting and sustainable drainage systems.
- **Safety and Quality:** Streets should be designed to be well-lit and pedestrian-friendly, incorporating appropriate traffic calming measures. Provide safe, accessible links to parks and natural areas, enhancing community well-being and local biodiversity.



**Figure 30:** A view of good example of arrangement of open spaces frontages

## 3.5 SAT 4: Employment Areas

### 3.5.1 Defining the area

The Employment Areas (SAT 4) of Fleckney are located primarily to the south and east of the village, along Saddington Road and Churchill Way, and are primarily composed of industrial developments. This area has evolved to accommodate a variety of employment and production-related activities, with a focus on small-scale industries and agricultural uses that contribute to Fleckney's local economy.

Unlike the residential areas of the village, SAT 4 is characterised by a more utilitarian and functional urban form, with buildings that cater to industrial, commercial, and agricultural needs. The location at the edge of the village and proximity of these areas to Fleckney's transportation networks, including roads like Saddington Road, makes them ideal for local economic activities that require access to logistics and distribution channels.



### SAT 4: Employment area qualities and features

<b>Connection</b>	Movement networks	Accessed from Saddington Road, provides direct access to the industrial areas, which consists of Churchill Way, and connect efficiently to the surrounding roads, facilitating transportation and logistics for local businesses.
<b>Built form</b>	Urban form	Primarily industrial, with functional and utilitarian buildings arranged along a grid-like street pattern, along Churchill Way, designed to accommodate the needs of local businesses and industries.
	Building lines, boundaries and setbacks	Buildings along Churchill Way are set close to the road to maximise space for large-scale operations, with minimal setbacks, often defined by industrial fences or low barriers marking boundaries.
	Building, size, scale and type	Features large, single-story industrial buildings and warehouses, with structures designed for commercial and production purposes, typically constructed from steel, brick, or corrugated metal to suit the needs of businesses like agriculture and light manufacturing.
<b>Nature</b>	Green and blue infrastructure	Green infrastructure is minimal, with some tree planting along Saddington Road and Churchill Way, but the area is primarily focused on utilitarian spaces, with few dedicated green or water management features integrated into the industrial landscape.
	Open spaces and biodiversity	There is limited open space, but small, unused patches of land and the occasional planted verge, particularly near Churchill Way, provide some opportunity for local biodiversity, though they are mainly functional and underutilised.
<b>Activity</b>	Uses and community	Predominantly made up of employment and industrial uses, contributing to Fleckney's local economy by hosting agricultural and light industrial businesses, though it lacks residential or community-focused amenities.

**Table 05:** Summary of the distinctive qualities and features that supplement the character of this Area Type.

### 3.5.2 Key Considerations and Good Features

#### Employment visual impact

The employment areas in Fleckney are mainly situated to the southern east part of the village. The employment areas are mainly located along Saddington Road and Churchill Way Industrial Estate. These zones host various businesses, contributing significantly to the local economy. The industrial estate on Churchill Way, offering a variety of job opportunities within the village. Some warehouses and shops are situated near to residential areas. Effects of these employment areas including noise, visual disturbance and traffic should be strictly checked and managed.



**Figure 32:** Views of employment areas

#### Design Code 8: Building layout and groupings

- **Proposals for new industrial developments should avoid creating access conflicts with surrounding residential areas.**
- **Building height and mass should not create abrupt changes in proximity to existing residential areas, but should be integrated within the surrounding context.**

## Employment Buildings and Industrial Units

The Guidance below aims to guide the potential inclusion of employment and light industrial units in the industrial areas. These typologies tend to be highly visible and thus will require to be treated with sensitivity towards the more traditional pattern of the parish.



**Figure 33:** Example of Industrial units.

### Design Code 9: Building architecture and appearance

- **New buildings should provide facade solutions which are visually attractive from the street and are engaging and respectful of the streetscape.**
- **The design of new buildings in the industrial area should be consistent in scale with nearby industrial buildings.**
- **New developments should be attractively designed and use high-quality contemporary building forms and materials.**
- **Buildings adjacent to open space areas and residential land uses should use a transitional scale and appearance to interface the adjoining environs.**
- **Parking and servicing areas should not dominate the area and should be screened by vegetation and mature trees and, where possible, be located to the rear of buildings.**

### Design Code 10: Boundary treatment and screening

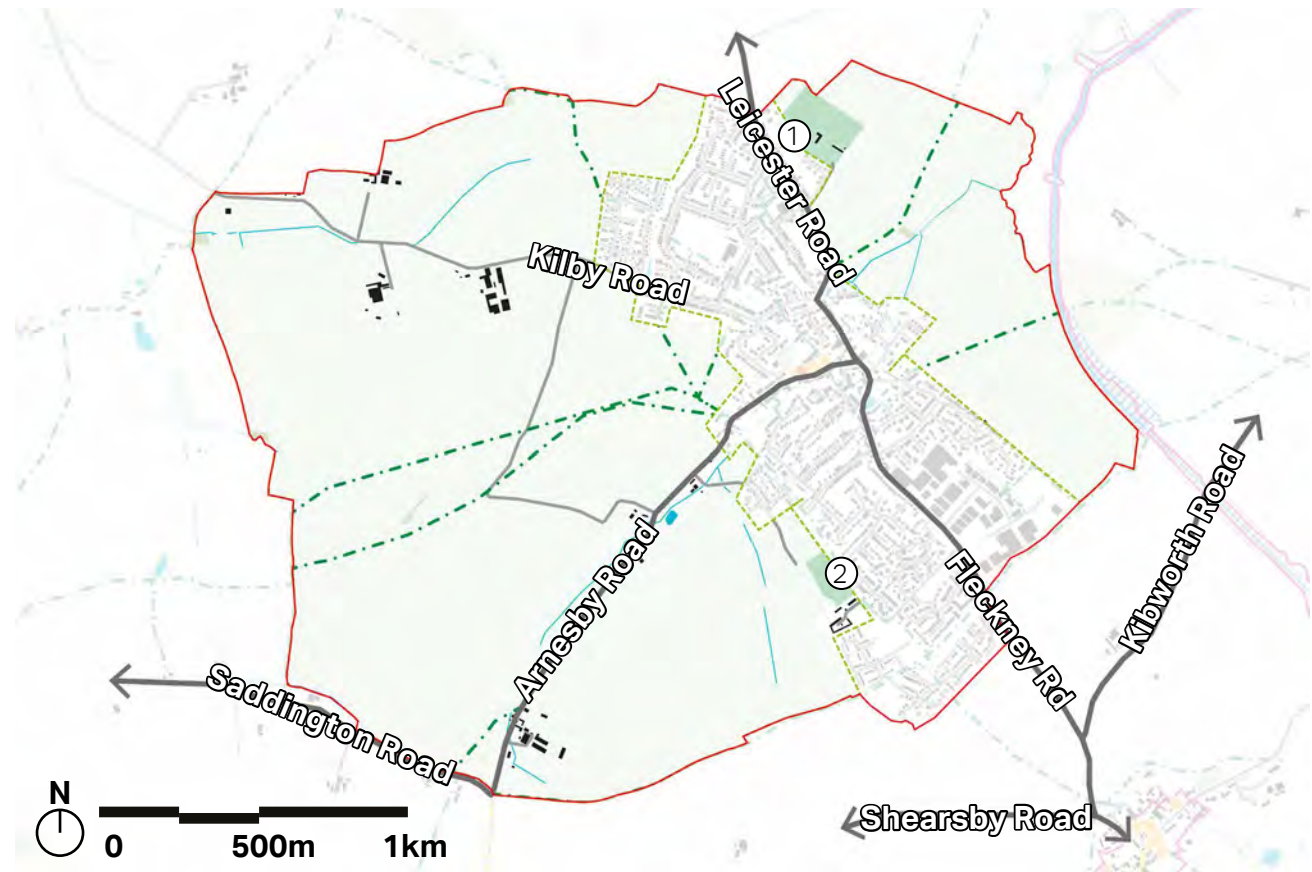
- **Buildings should be well set back from main roads to provide opportunity for landscape planting to improve the visual quality of the streetscape.**
- **Boundary treatment for new developments should be designed to frame the building and improve the overall streetscape.**
- **Plot boundaries should be screened with native vegetation or other landscape design solutions.**

## 3.6 CAT A: Open Countryside











### 3.6.1 Defining the area

Open Countryside (CAT A) encompasses all the areas outside the Settlement Area Types (SATs) but within the Fleckney neighbourhood area, forming the rural backdrop to the village. This land is characterised by expansive fields, agricultural land, farm buildings and natural landscapes, offering a contrast to the main settlement areas of Fleckney.

The open countryside plays a vital role in maintaining the village's rural ambiance, providing scenic views, recreational opportunities, and supporting local biodiversity. It also serves as an important buffer between the village's built environment and the wider agricultural landscape, helping to mitigate the visual impact of urban development. The preservation of this open countryside is key to maintaining the distinctive character of Fleckney, ensuring the village retains its connection to its agricultural roots and the surrounding natural environment.



#### KEY

	NA boundary		Greenspace		Roads
	Grade II* listed		Woodland		PRoW
	Grade II listed		Waterbodies		Recreation ground
					Playing field



**Figure 34:** Rural lanes and adjacent open countryside fields.



**Figure 35:** There is an extensive network of PRow.



**Figure 36:** Open countryside maintains the village's rural ambiance.

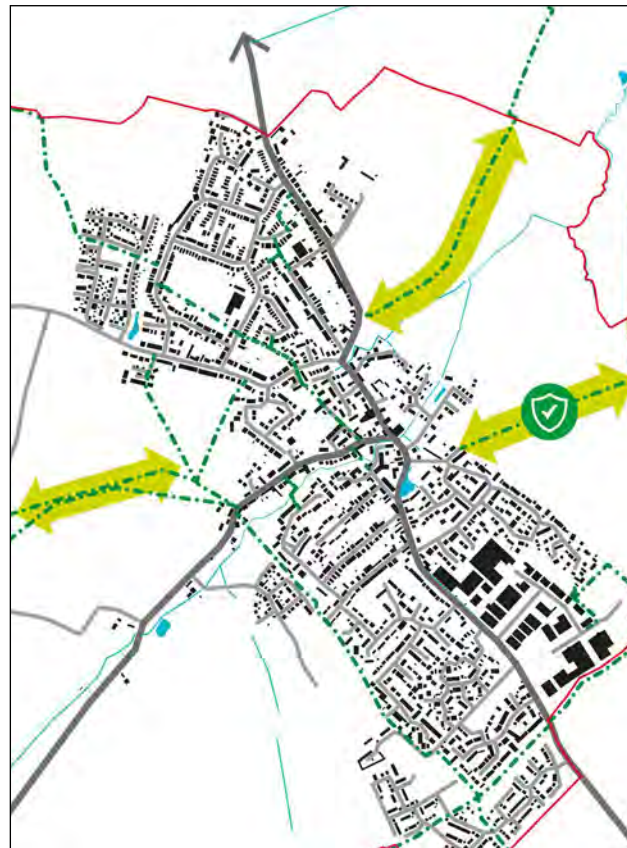
### 3.6.2 Key issues affecting design and infrastructure

#### Green corridors



Fleckney's green corridors provide essential links between open spaces, supporting biodiversity and recreation. These corridors help define the village's character and maintain its rural feel. The surviving ridge and furrow fields, remnants of medieval agriculture, contribute to the village's historic landscape and should be considered in future land use planning.



**Figure 38:** There is a good network of open spaces



**Figure 37:** Green corridors

-  Main village leisure routes
-  Ridge and furrow to protect

### Design Code 11: Open Countryside and Green Corridors

- The open countryside should remain predominantly agricultural, with minimal development permitted to preserve the rural character of the area;
- Any new buildings or structures must be designed to blend with the rural environment, using materials such as natural stone, timber, and other local materials that are sympathetic to the landscape;
- Development proposals must carefully assess the visual impact on the surrounding landscape and Green Corridors; ensure that they do not harm long-range views from public rights of way or the village; and
- Any new infrastructure, such as footpaths or bridleways, should enhance the accessibility of the countryside for recreation, ensuring connectivity with Fleckney.

## Treatment of Woody Species

Fleckney contains various woodlands and trees which improve both its natural environment and visual beauty. The woodland areas support different species of wildlife while giving local residents access to outdoor recreational spaces. The green areas face threats from ash dieback disease which results from a fungal infection that damages ash tree populations. The environmental preservation requires careful management because Fleckney stands near both Saddington conservation area and the Grand Union Canal. The protection of Fleckney's woodland heritage depends on establishing Tree Preservation Orders and involving the community in conservation initiatives.

### Design Code 12: Woodland, Trees and Hedgerows

- **Developments should be designed to retain trees, particularly those of landscape and biodiversity importance, with a view to increasing tree cover.**
- **The spacing of development should reflect the rural character and allow for long distance views of the countryside from the public realm. Trees and landscaping should be incorporated in the design.**
- **In the outer NA, the rural character should be preserved and enhanced through the retention of grass verges, hedgerows and trees and new plantings to improve biodiversity.**
- **Species choice should be predominantly native but not completely; a 2:1 ratio would be appropriate to help build a tree population that supports UK wildlife but is also capable of responding to new diseases and climate threats.**
- **Provision of parks, allotments, green links, open green spaces and any proposals by which local residents can connect more with the natural environment, even in the village centre, are encouraged by any potential development.**

A photograph of a row of brick terraced houses under an overcast sky. A large green circle is overlaid on the center of the image, containing white text. The houses are made of red brick with white window frames and door surrounds. Some houses have small gabled porches. A tall brick chimney with a television antenna is visible on the roof of one of the houses.

**Area wide guidance  
and codes**

**04**

## 4. Area wide guidance and codes

**This section supports decision-makers and designers when producing or reviewing planning applications within the entire NA. This applies to development in allocated sites, infill development and windfall development that may come forward, with a focus on proposed residential development.**

It is acknowledged that there is not always agreement on aesthetic issues and opinions may vary. The following guidance and codes therefore allows for flexibility and design innovation, whilst ensuring that any new development is appropriate and complementary to the surrounding context.

To enable a clear design process, new development proposals must use the this section to ensure that development proposals enhance the setting and sustainability of Fleckney, while not detracting from its context, local character and sense of place.

### 4.1 Guidance and code themes

The Guidance outlined in this chapter aim to apply to the whole of the NA. These have been derived from current urban design best practice and are considered essential for a successful development.

These Guidance advocate the use of context for design cues. In this sense it is expected that a design proposal will make reference to different design elements such as layout of buildings, building envelope, materials, building forms, colours, roofs and fenestrations.

These guidance and codes were decided based on meeting with the Fleckney Parish Council as well as an in-depth desktop study of the NA. Each of these themes will be accompanied by relevant supported analysis, photographs and diagrams.

Codes and guidance are arranged under the following pages.

## 4.2 Infill development

Infill development is smaller scale development (generally fewer than 10 homes) within an existing urban and developed context. This type of development commonly consists of three main types:

- Gap site development within a street frontage;
- Backland development; and
- Site redevelopment (for example, replacement of existing building/s).

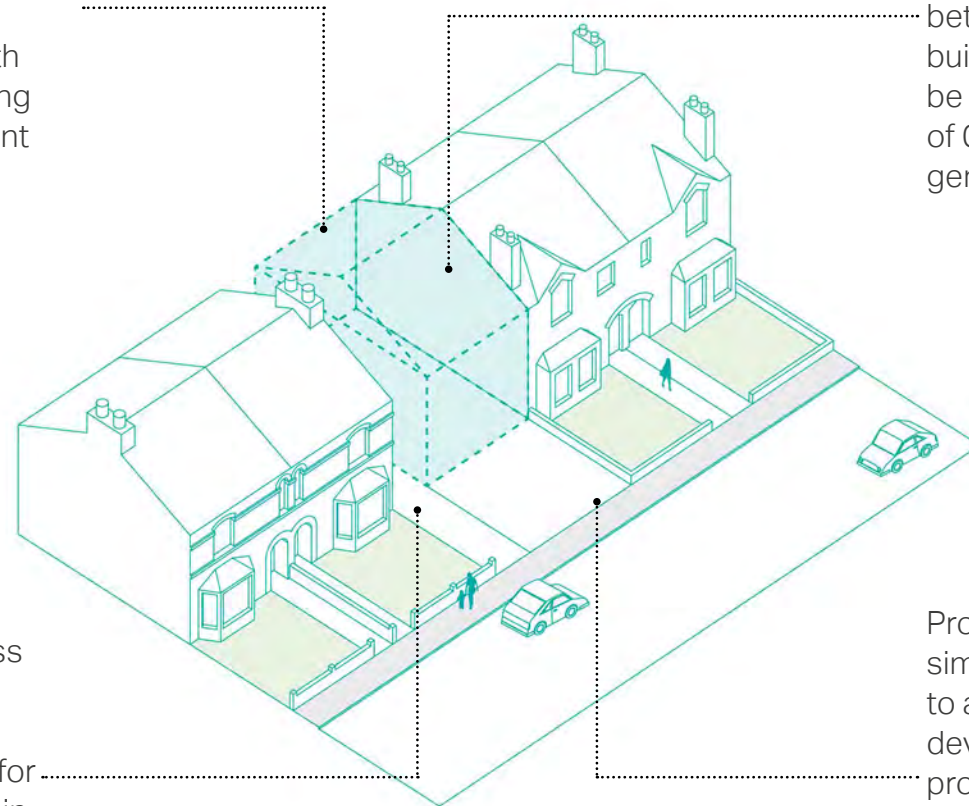
The overarching aim of the Design Code is to promote context-sensitive infill housing of a high quality. This should help reinforce local character and create sustainable growth in Fleckney.

### Design Code 13: Infill Development

- **Scale and massing: Building scale and massing should be in keeping with the prevailing development pattern and not be overbearing on existing properties or deprive them of light, including overlooking or overshadowing of both windows and amenity space.**
- **Enclosure: Building scale and position on plot should help to define and enclose the space within the street corridor or square to an appropriate degree based on the existing street section (building to building) and level of enclosure (ratio of street width to building height).**
- **Fenestration (window pattern): The positioning of windows should be in keeping with the predominant positive building character on the street or harmonise with adjacent buildings of good character.**
- **Access: Building entrances should address the street with a main access and main frontage. Corner buildings should address both streets with frontages but the main entrance could be on either subject to access requirements.**
- **Building heights: Building heights should be guided by the development's character area. A variable eaves line and ridge line is allowed to create interest but variation between adjacent buildings should be a maximum of 0.5 storeys in general.**
- **Refuse and cycle storage: Access for bin and cycle storage should be provided with stores being integrated within plot boundaries. Snickets / alleyways should be considered for terraced buildings with four or more units in order to allow access to the rear of properties for cycle and bin storage.**

- **Parking provision:** Parking should be integrated on plot where possible with parking spaces set behind the building line, generally to the side of plot being preferable. For narrow dwellings it is preferred to retain a small front garden with a boundary wall as opposed to an open hard surface parking space. Where parking is required to the front of the plot it should be afforded sufficient space and utilise hedgerows to screen cars from the street.
- **Proportionate backland development:** In the event of backland development, proposals should ensure that the density, scale and appearance reflect the immediate context (i.e. the original dwelling). Backland development should not be larger in height, massing or scale than the existing dwelling. The privacy, integrity and amenity of the existing dwelling must be protected from that proposed on the backland.

Scale and massing in keeping with the prevailing development pattern.



Variation between adjacent buildings should be a maximum of 0.5 storeys in general.

Snickets / alleyways allow access to the rear or terraced properties for cycle and bin storage.

Providing a similar setback to adjacent development produces a cohesive building line.

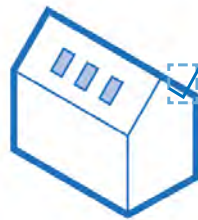
**Figure 39:** Contextual infill development diagram.

## 4.3 Extension and conversion

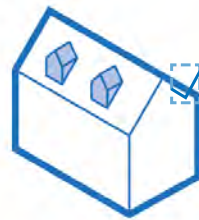
There are a number of Guidance principles that residential extensions and conversions should follow to maintain character:

- The original building should remain the dominant element of the property regardless of the scale or number of extensions. The newly built extension should not overwhelm the building from any given viewpoint;
- Designs that wrap around the existing building and involve overly complicated roof forms should be avoided; and
- The pitch and form of the roof used on the building adds to its character and extensions should respond to this where appropriate.

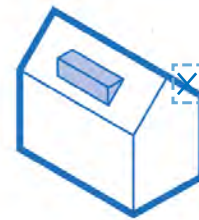
Design treatment in case of loft conversion:



Loft conversion incorporating skylights.



Loft conversion incorporating gable dormers.



Loft conversion incorporating a long shed dormer which is out of scale with the original building



Original roofline of an existing building



Loft conversion incorporating gable dormers.

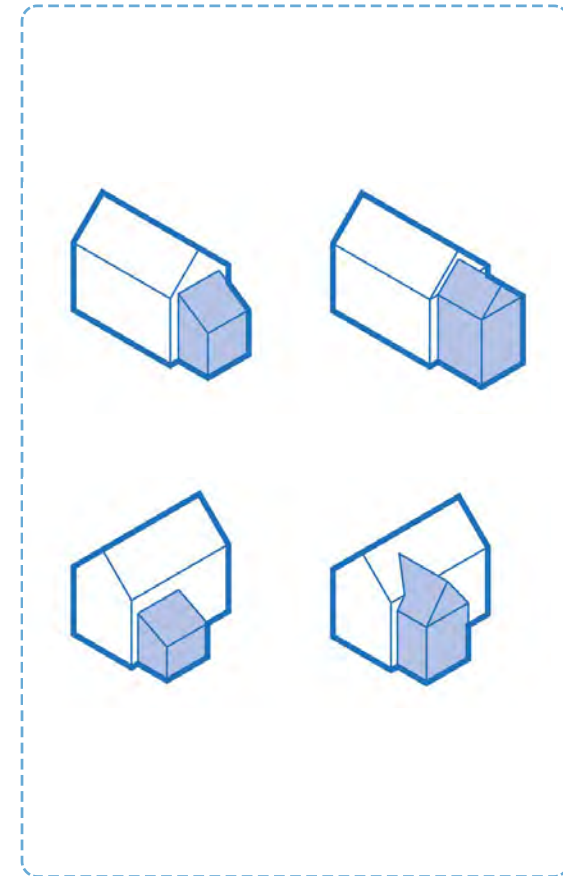


Loft conversion incorporating gable dormers which are out of scale and do not consider existing window rhythm or frequency,

## Design Code 14: Extension and Conversion

- Extensions should consider the materials, architectural features, window sizes and proportions of the existing building and respect these elements to design an extension that matches and complements the existing building.
- In the case of side extensions, the new part should be set back from the front of the main building and retain the proportions of the original building.
- In the case of rear extensions, the new part should not have a harmful effect on neighbouring properties in terms of overshadowing, overlooking or privacy issues.

- Many household extensions are covered by permitted development rights, and so do not need planning permission.
- Extensions should not result in a significant loss to the private amenity area of the dwelling.
- Any housing conversions should respect and preserve the building's original form and character.
- Where possible, reuse as much of the original materials as possible, or alternatively, use like-for-like materials. Any new materials should be sustainable and be used on less prominent building parts.



**Figure 40:** Good example for side extensions, respecting existing building scale, massing and building line.

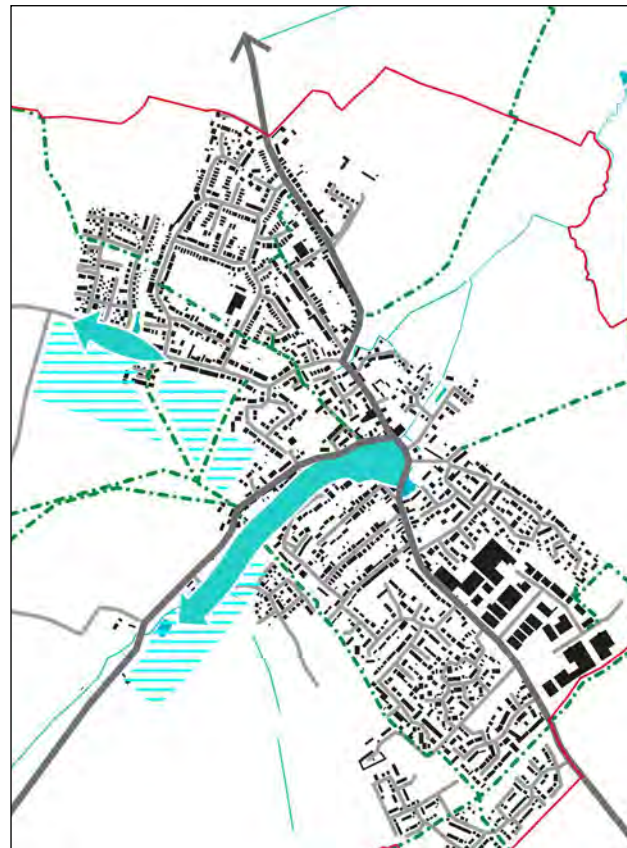
## 4.4 . Flood Resilience

### Flood risk

Several areas in Fleckney have experienced recent flooding or face ongoing risks due to natural springs and poor drainage. Roads and properties in low-lying areas are particularly vulnerable, with surface water pooling after heavy rainfall. Existing drainage infrastructure struggles to cope, increasing the need for flood mitigation measures.

Due to the floodable zone of the area, it is recommended that developments should keep surface water discharge rates to below the greenfield. Where this is not possible, this rate should be considered as the maximum.

Where possible, developments should incorporate Sustainable Drainage Systems (SuDS). These systems employ natural processes and create areas for residents to engage in recreational activities that promote interaction with plants and animals.



**Figure 41:** Flood risk

- ← Areas flooded recently
- ▨ High risk due to springs

### Design Code 15: Resilience to the Flood Risk Climate Emergency

All new development should work to moderate extremes of temperature, wind, humidity, local flooding and pollution within the neighbourhood area:

- Avoid siting homes in high risk flood areas and mitigate increased risk of storms and flooding with sustainable drainage systems (SuDS). These reduce the amount and rate at which surface water reaches sewers and watercourses. This reduces pressure on valuable water sources.
- Eco-systems cannot adapt as fast as the climate is changing, leading to loss of biodiversity. Protecting and enhancing woodlands, watercourses and green infrastructure can combat this. Use street trees and planting to moderate and improve micro-climates for streets and spaces.

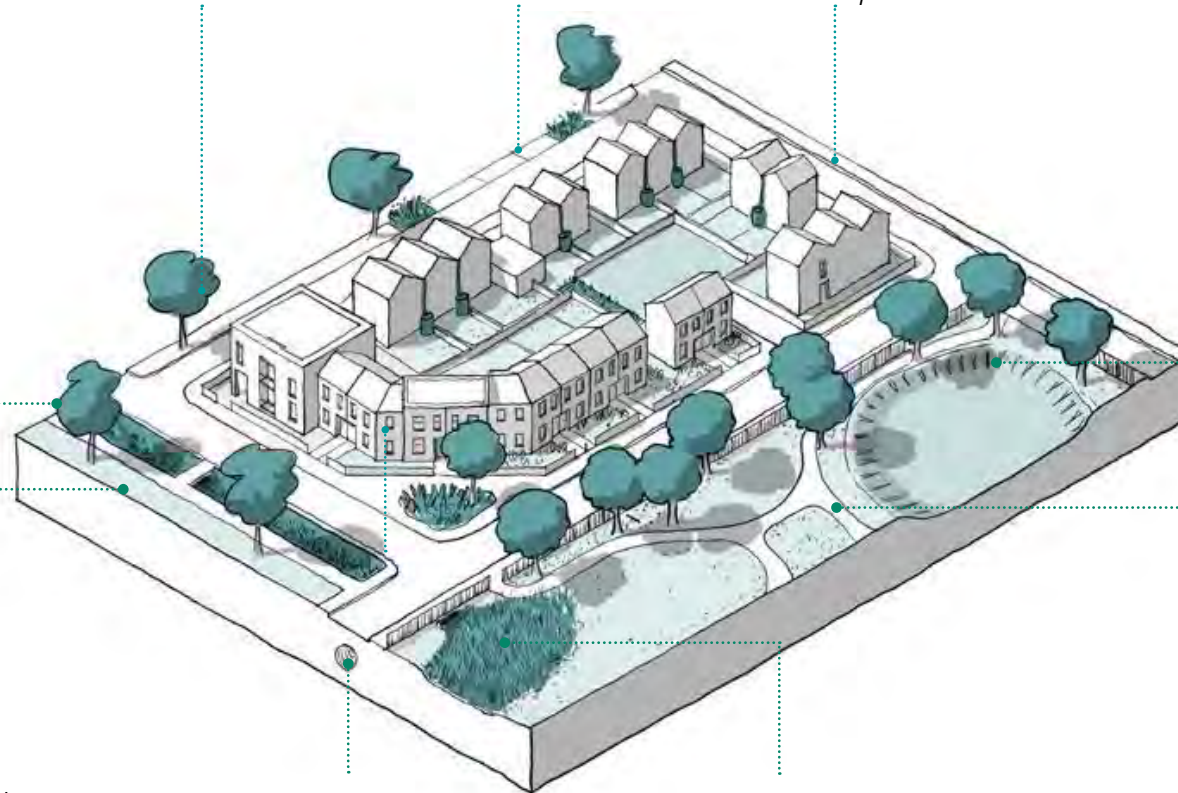
*Street tree planting: SuDS designed into highway provision can provide dual-use benefits when integrated with street tree provision.*

*Green roofs and walls: Provide capacity to hold and attenuate water run-off as well as ecological and leisure benefits.*

*Soakaways and filter drains: Shallow ditches and trenches filled with gravel or stones.*

*Rain capture: Water butts and other rainwater harvesting systems collect rainwater for use in gardens or for non-potable uses reducing water consumption.*

*Basins and ponds: Attenuation ponds that are normally dry but fill during a rain event and then either store or gradually discharge water to the system.*



*Swales: Shallow channels that provide attenuation while also channeling water to other features such as ponds.*

*Retention tanks: In high density schemes water can be attenuated in underground structures.*

*Reed beds and wetlands: Topography can be used to create wetlands that provide attenuation capacity as well as filtering out pollutants and providing habitat for wildlife.*

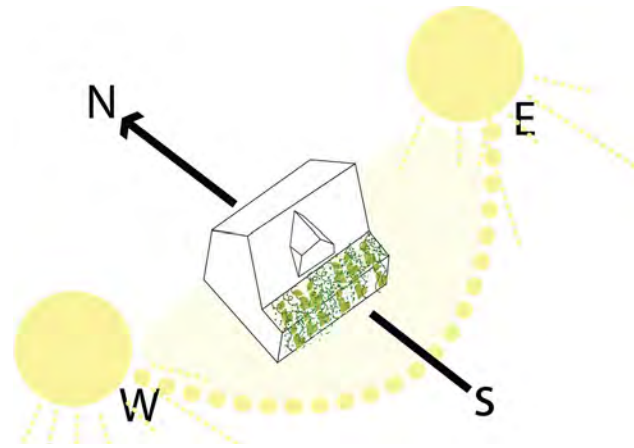
*Permeable surfacing: Surfaces that allow water to percolate into the ground including natural surfaces, gravel and low traffic volume engineered road surfaces and hard-standings in front gardens.*

**Figure 42:** Example of arrangement for resilience to the climate change

## 4.5 Sustainable Design

The Local Plan encourages creating buildings and spaces with reduced environmental impact, offering people opportunities to live lower carbon lifestyles. Buildings should be suitable for future adaptation, conversion or expansion. The sustainable design and construction of new buildings and extensions to existing buildings have an essential role in reducing running costs, improving energy efficiency, and reducing greenhouse gas emissions.

New housing and employment developments must clearly show how utility services will be provided, ensuring utilities are involved early in the planning stage. Buildings must be designed to facilitate future adaption integration of sustainability from the concept stage, with consideration of passive solar heating, cooling and energy efficient strategies. The energy hierarchy should be adopted through implementation of passive environmental design principles, then specification of energy efficient building services before the incorporation of renewable energy sources. All new buildings must have integrated roof mounted PV panels or tiles.



**Figure 43:** Precedent images - Examples of energy-efficiency design.

### Design Code 16: Energy Saving

- The design of buildings for energy efficiency should be consistent with the Government's current zero carbon buildings policy as outlined in the NPPF and the Code for "Sustainable Homes and Energy Performance of Buildings" (MHCLG publication) or any replacement, with the aim to achieve the highest viable level of energy conservation.
- The measures taken towards a zero carbon development must be identified in a Design Statement.
- Where an energy performance certificate is required for a building the target SAP (Standard Assessment Procedure) rating aimed for in the design must be shown in a Design Statement.
- Where buildings are designed to the standards in the "Code for Sustainable Homes and Energy Performance of Buildings", the anticipated star rating to be achieved under the initiative must be included in a Design Statement.

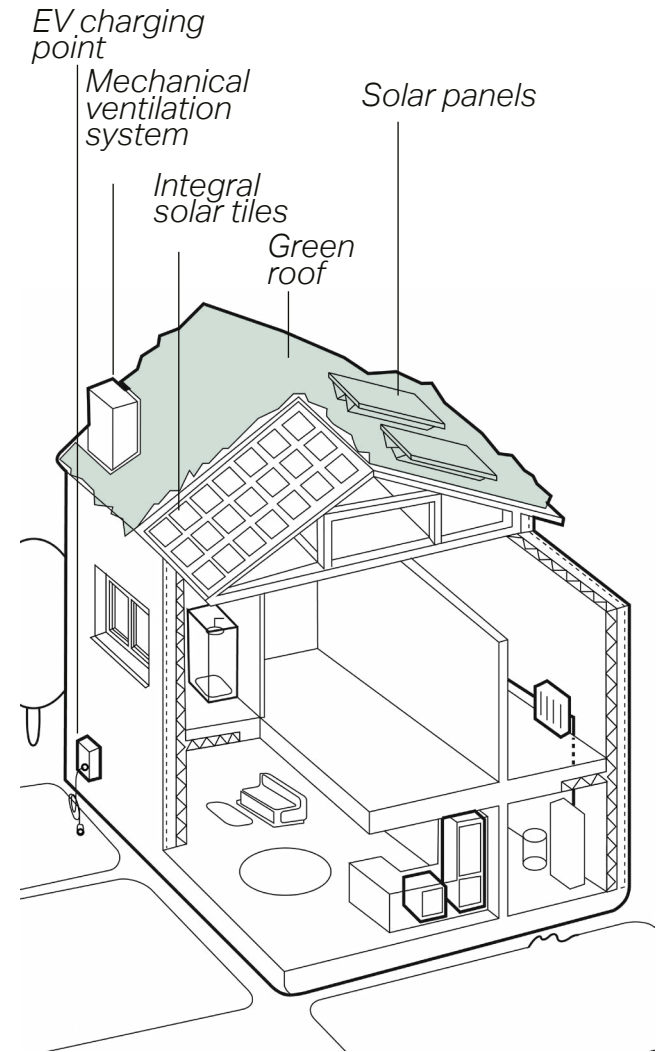
- By default, new development should adopt a fabric first approach in line with the governments emerging Future Homes Standard, to attain higher standards of insulation and energy conservation.
- Reducing energy demand further by employing passive design Guidance for homes is desirable and can make some forms of development more acceptable to the community (window orientation, solar gain, solar shading, increased insulation, ventilation with heat-recovery).
- Maximise on-site renewable energy generation (solar, ground source, air source and wind driven).
- Consider building form and thermal efficiency: point-block/ terraced / semi-detached / detached all have different energy efficiency profiles. This must be balanced with local design preference and character considerations to ease acceptance for development.



**Figure 46:** Air source heat pump unit located to the rear / side elevation of a dwelling, so to avoid its visual impact on the dwellings frontage and wider streetscape.



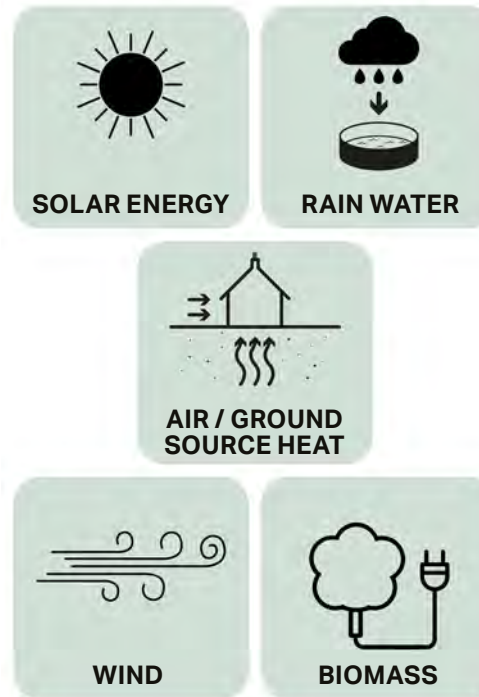
**Figure 44:** Precedent image - example of energy-efficiency design.



**Figure 45:** Cut-through diagram of an energy-efficient home and its features.

## Design Code 17: Energy Efficiency

- Active measures may include the specification of energy efficient building services and controls to facilitate efficient operation.
- All heated pipes and ducts should be insulated, and service penetrations sealed, to improve system efficiency, prevent heat loss and minimise the risk of overheating.
- Lighting in the commercial buildings should be on zone control with presence and daylight detection where suitable. LED light fittings should be specified, both internally and externally, with automatic switch off at night where not required for safety or security.



**Figure 47:** Key alternative natural energy sources

## Design Code 18: Electric Vehicle Charging

Current transition to electric vehicle technology and ownership comes with related issues that must be addressed by new development. Two key areas are explored below - public parking areas and private parking for homes.

Design issues to address for public parking:

- Provision of adequate new charging points and spaces and retrofitting existing parking areas.
- Serving remote or isolated car parks (e.g. in woodland areas).
- Retrofitting existing public parking and upkeeping design quality of streets and spaces (attractiveness and ease of servicing/maintenance).

- Integrating charging infrastructure sensitively within streets and spaces, for example, by aligning with green infrastructure and street furniture.



**Figure 48:** Public electric vehicles charging points



**Figure 49:** Home electric vehicles charging point

#### Design issues to address for parking at the home:

- Convenient on plot parking and charging points close to homes.
- Potential to incorporate charging points under cover within car ports and garages.
- Still need to integrate car parking sensitively within the streetscene. For example, parking set behind the building line or front of plot spaces lined with native hedgerow planting.
- Need to consider visitor parking /charging needs.
- Existing unallocated / on-street parking areas and feasibility to provide electric charging infrastructure not linked to the home.
- Potential for providing secure, serviced communal parking areas for higher density homes.



Next Steps

05

## 5. Next Steps

This document provides a series of design Guidance, design codes and recommendations for the Fleckney Neighbourhood Area. The document is based on high-level analysis regarding the context, constraints, history and characteristics of the village and surrounding countryside areas. The analysis suggest that any future development should be in-line with the local characteristics and existing context. The design codes provided within the document will guide future developments across the whole NA to respect, conserve and improve the existing character, heritage, links, and villagescape features.

Fleckney Parish Council is recommended to use this document to embed design policies within the Neighbourhood Plan to achieve the objectives set out in this document. Developers should also observe this document to understand the design quality they are expected to achieve within the NA.

We would like to thank the Fleckney Parish Council for their efforts in assisting with the content of this report.

## About AECOM

AECOM is the world's trusted infrastructure consulting firm, delivering professional services throughout the project lifecycle — from planning, design and engineering to program and construction management. On projects spanning transportation, buildings, water, new energy and the environment, our public- and private-sector clients trust us to solve their most complex challenges. Our teams are driven by a common purpose to deliver a better world through our unrivaled technical expertise and innovation, a culture of equity, diversity and inclusion, and a commitment to environmental, social and governance priorities. AECOM is a *Fortune 500* firm and its Professional Services business had revenue of \$13.2 billion in fiscal year 2020. See how we are delivering sustainable legacies for generations to come at [aecom.com](https://www.aecom.com) and [@AECOM](https://www.instagram.com/AECOM).